

# Notice of Planning Committee

Date: Thursday, 11 June 2020 at 1.00 pm

Venue: Skype Meeting



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## Membership:

### Chairman:

Cllr S Bull

### Vice Chairman:

Cllr S McCormack

Cllr S Bartlett

Cllr S Baron

Cllr M Davies

Cllr B Dunlop

Cllr P R A Hall

Cllr P Hilliard

Cllr T Johnson

Cllr D Kelsey

Cllr M Le Poidevin

Cllr D Mellor

Cllr T O'Neill

Cllr A M Stribley

Cllr T Trent

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## NOTE: Membership subject to any changes following Annual Council on 9 June 2020.

All Members of the Planning Committee are summoned to attend this meeting to consider the items of business set out on the agenda below.

The press and public are welcome to view the live stream of this meeting at the following link:

<https://democracy.bcpCouncil.gov.uk/ieListDocuments.aspx?MIId=4393>

If you would like any further information on the items to be considered at the meeting please contact: Democratic Services or email [democratic.services@bcpCouncil.gov.uk](mailto:democratic.services@bcpCouncil.gov.uk)

Press enquiries should be directed to the Press Office: Tel: 01202 454668 or email [press.office@bcpCouncil.gov.uk](mailto:press.office@bcpCouncil.gov.uk)

This notice and all the papers mentioned within it are available at [democracy.bcpCouncil.gov.uk](https://democracy.bcpCouncil.gov.uk)

GRAHAM FARRANT  
CHIEF EXECUTIVE

3 June 2020



Available online and  
on the Mod.gov app



## Maintaining and promoting high standards of conduct

### Declaring interests at meetings

Familiarise yourself with the Councillor Code of Conduct which can be found in Part 6 of the Council's Constitution.

Before the meeting, read the agenda and reports to see if the matters to be discussed at the meeting concern your interests

Do any matters being discussed at the meeting relate to your registered interests?

Disclosable Pecuniary Interest

Yes

Declare the nature of the interest

Do NOT participate in the item at the meeting. Do NOT speak or vote on the item EXCEPT where you hold a dispensation

You are advised to leave the room during the debate

Local Interest

Yes

Declare the nature of the interest

Applying the bias and pre-determination tests means you may need to refrain from speaking and voting

You may also need to leave the meeting. Please seek advice from the Monitoring Officer

No

Do you have a personal interest in the matter?

Yes

Consider the bias and pre-determination tests

You may need to refrain from speaking & voting

You may also need to leave the meeting. Please seek advice

No

You can take part in the meeting speak and vote

What are the principles of bias and pre-determination and how do they affect my participation in the meeting?

Bias and predetermination are common law concepts. If they affect you, your participation in the meeting may call into question the decision arrived at on the item.

#### Bias Test

In all the circumstances, would it lead a fair minded and informed observer to conclude that there was a real possibility or a real danger that the decision maker was biased?

#### Predetermination Test

At the time of making the decision, did the decision maker have a closed mind?

If a councillor appears to be biased or to have predetermined their decision, they must NOT participate in the meeting.

For more information or advice please contact the Monitoring Officer  
(anne.brown@bcpcouncil.gov.uk)

### Selflessness

Councillors should act solely in terms of the public interest

### Integrity

Councillors must avoid placing themselves under any obligation to people or organisations that might try inappropriately to influence them in their work. They should not act or take decisions in order to gain financial or other material benefits for themselves, their family, or their friends. They must declare and resolve any interests and relationships

### Objectivity

Councillors must act and take decisions impartially, fairly and on merit, using the best evidence and without discrimination or bias

### Accountability

Councillors are accountable to the public for their decisions and actions and must submit themselves to the scrutiny necessary to ensure this

### Openness

Councillors should act and take decisions in an open and transparent manner. Information should not be withheld from the public unless there are clear and lawful reasons for so doing

### Honesty & Integrity

Councillors should act with honesty and integrity and should not place themselves in situations where their honesty and integrity may be questioned

### Leadership

Councillors should exhibit these principles in their own behaviour. They should actively promote and robustly support the principles and be willing to challenge poor behaviour wherever it occurs

# AGENDA

Items to be considered while the meeting is open to the public

**1. Election of Chairman of the Planning Committee**

Councillors are asked to elect the Chairman of the Planning Committee for the 2020/21 Municipal Year.

**2. Election of Vice Chairman of the Planning Committee**

Councillors are asked to elect the Vice Chairman of the Planning Committee for the 2020/21 Municipal Year.

**3. Apologies**

To receive any apologies for absence from Members.

**4. Substitute Members**

To receive information on any changes in the membership of the Committee.

Note – When a member of a Committee is unable to attend a meeting of a Committee or Sub-Committee, the relevant Political Group Leader (or their nominated representative) may, by notice to the Monitoring Officer (or their nominated representative) prior to the meeting, appoint a substitute member from within the same Political Group. The contact details on the front of this agenda should be used for notifications.

**5. Declarations of Interests**

Councillors are requested to declare any interests on items included in this agenda. Please refer to the workflow on the preceding page for guidance.

Declarations received will be reported at the meeting.

**6. Confirmation of Minutes**

To confirm and sign as a correct record the minutes of the Meeting held on 21 May 2020.

7 - 14

**7. Public Issues**

To receive any requests to speak on planning applications which the Planning Committee is considering at this meeting.

The deadline for the submission of requests to speak is 12 noon on Wednesday 10 June. Requests should be submitted to Democratic Services using the contact details on the front of this agenda.

Further information is contained in the Protocol for Public Speaking at Planning Committee which is available in the PDF that forms part of this item.

15 - 18

## 8. Schedule of Planning Applications

19 - 28

To consider the planning applications as listed below.

See planning application reports circulated at 8a-8e, as updated by the agenda addendum sheet to be published on 10 June 2020.

The running order in which planning applications will be considered will be as listed on this agenda sheet. Timings shown are approximate and included as a guide only.

The Chairman retains discretion to propose an amendment to the running order at the meeting if it is considered expedient to do so, but applications will not be considered earlier than the published time.

Members will appreciate that the copy drawings attached to planning application reports are reduced from the applicants' original and detail, in some cases, may be difficult to read. The submitted drawings can be viewed on the application file at the relevant local planning office or by using the relevant planning register for this meeting, online at:

<https://planning.christchurchandeastdorset.gov.uk/search.aspx?auth=1&AspxAutoDetectCookieSupport=1>

<https://www.bournemouth.gov.uk/planningbuilding/CurrentPlanningApplications/PlanningApplicationRegister.aspx>

<https://www.poole.gov.uk/planning-and-building-control/planning/planning-applications/find-a-planning-application/>

Councillors are advised that if they wish to refer to specific drawings or plans which are not included in these papers, they should contact the Case Officer at least 24 hours before the meeting to ensure that these can be made available.

Development Plans for the BCP Council area are available to view online at:

<https://www.bournemouth.gov.uk/planningbuilding/PlanningPolicy/Local-Plan-Documents/Local-Plan-Documents.aspx>

<https://www.poole.gov.uk/planning-and-building-control/planning-policy-and-guidance/>

<https://www.christchurch.gov.uk/planning-buildings-land/planning-policy/christchurch/christchurch-borough-council-local-plan.aspx>

### a) 10 Suffolk Road, Bournemouth

29 - 58

Central Ward

7-2019-6638-J

Outline submission for the erection of a 3 to 6 storey building of 31 flats with



	basement parking and formation of vehicular access.	
b)	<b>The Goods Yard, 14 Station Approach, Broadstone, BH18 8AX</b> (Broadstone)  APP/19/00416/F  Side and rear extension to form two additional apartments together with associated access and parking and provision of refuse collection area.	59 - 76
c)	<b>81-83 Hankinson Road, Bournemouth, BH9 1HP</b> (Winton East)  7-2019-20654-B  Erection of bungalow and formation of parking spaces.	77 - 94
d)	<b>134 High Street, Poole, BH15 1DN</b> (Poole Town)  APP/19/01563/F  Change of Use of part ground floor, part first floor, and second and third floors from D1 Use to create 6no. flats (C3 Use)	95 - 108
e)	<b>15 Creedy Drive Christchurch BH23 1NX</b> (Christchurch Town)  8/20/0137/HOU  Single storey rear extensions. New decking with balustrade.	109 - 122

No other items of business can be considered unless the Chairman decides the matter is urgent for reasons that must be specified and recorded in the Minutes.

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**BOURNEMOUTH, CHRISTCHURCH AND POOLE COUNCIL**  
**PLANNING COMMITTEE**

Minutes of the Meeting held on 21 May 2020 at 1.00 pm

Present:-

Cllr S Bull – Chairman

Cllr S McCormack – Vice-Chairman

Present: Cllr S Bartlett, Cllr S Baron, Cllr M Davies, Cllr B Dunlop,  
Cllr P R A Hall, Cllr P Hilliard, Cllr T Johnson, Cllr D Kelsey,  
Cllr M Le Poidevin, Cllr D Mellor, Cllr T O'Neill, Cllr A M Stribley and  
Cllr T Trent

Also in  
attendance:

150. Apologies

No apologies for absence were received.

151. Substitute Members

There were no substitute members.

152. Declarations of Interests

In relation to items 6d and 6e Cllr D Mellor disclosed for transparency that he is the Chair of trustees for Stormbroke Charity and a governor of Bishop Aldhelm's school. Cllr D Mellor explained that both had received funding from the Talbot Village Trust.

In relation to items 6d and 6e Cllr S McCormack disclosed that he is a trustee of the Somerford ARC Community Centre which has received funding from the Talbot Village Trust. Cllr S McCormack confirmed he would exclude himself from participating in items 6d and 6e.

In relation to items 6d and 6e Cllr T Trent disclosed that he is a trustee of an organisation that had received funding from the Talbot Village Trust. Cllr T Trent confirmed that he would exclude himself from participating in items 6d and 6e.

153. Confirmation of Minutes

The minutes of the meeting held on 30 April 2020 were confirmed and signed as an accurate record.

154. Public Issues

There were a number of written statements received from members of the public, applicants and their representatives, and Ward Councillors, which were confirmed as received as each application was considered.

Furthermore, the Chairman exercised his discretion to allow Ward Councillors to attend and speak. Several Ward Councillors made verbal statements, as follows:

Cllr M Brooke – Item 6c  
Cllr K Rampton - Item 6d/6e  
Cllr P Broadhead – Item 6d/6e  
Cllr M Greene – Item 6f

155. Schedule of Planning Applications

The Committee considered planning application reports, copies of which had been circulated and which appear as Appendices A – F to these minutes in the Minute Book. Further to this the Committee received an update sheet in relation to the applications, a copy of which had been circulated and appears as Appendix G to these minutes in the Minute Book. The Committee considered the planning applications as set out in Minutes 156 – 162 below.

156. 19 Avon Wharf Bridge Street Christchurch BH23 1DJ

(Christchurch Town)

8/20/0079/HOU

To replace existing roof lights with dormers to front and rear. To erect conservatory to rear.

Written representations received:

Ø IN OBJECTION –

R Deeming

T Lane

P Fenning

Ø IN SUPPORT –

None received

Ø WARD COUNCILLOR –

Cllr M Cox

Verbal Representations received:

Ø WARD COUNCILLOR:

None

During the debate an alternative Motion to refuse Planning Permission was LOST.

Voting:

For – 4      Against – 8      Abstain – 2

**RESOLVED that the application be granted Planning Permission in line with recommendation as set out in the report.**

For – 9      Against – 4      Abstain – 1

NOTE:

One Councillor did not vote due to technical difficulties.

Cllr P Hall requested that his vote Against the resolution be recorded.

157. 7 Malmesbury Close Christchurch BH23 1NU

(Christchurch Town)

8/20/0046/HOU

Construct Bin Storage at the front of Property (Retrospective)

Written Representations received:

Ø IN OBJECTION -

K Beresford

C Martin

Ø IN SUPPORT -

A King

Ø WARD COUNCILLOR:

None

**RESOLVED that:** the Application be granted Planning Permission in line with recommendation as set out in the report.

For – 9      Against – 5      Abstain – 0

NOTE: One Councillor did not vote due to technical difficulties.

Cllr P Hall requested that his vote Against the resolution be recorded.

158. The Goods Yard, 14 Station Approach, Broadstone, BH18 8AX

(Broadstone)

APP/19/00414/P

Major Outline application for the erection of 33 apartments together with associated access and parking.

Written Representations received:

Ø IN OBJECTION -  
None

Ø IN SUPPORT -  
R Chapman  
G Moir

Ø WARD COUNCILLOR:  
None

Verbal Representations received:

Ø WARD COUNCILLOR:  
None

**RESOLVED that the application be granted Planning Permission in line with recommendation as set out in the report.**

For – 11                  Against – 4                  Abstain – 0

159. Land at Highmoor Farm, Purchase Road, Talbot Village, Poole

(Talbot and Branksome Woods)

APP/20/00095/F

Conversion of agricultural barn to digital exchange building including alterations to form three doorways.

Written Representations received:

Ø IN OBJECTION:  
A Barraclough  
K Lingane  
M Gardner  
N Dobbs  
S Coltman and the Talbot Woods Residents Association  
G Tuffin and the Talbot Village Residents Association/Neighbourhood Watch

Ø IN SUPPORT:  
J Gibson-Fleming  
G Fong  
T Harris  
S Trueick

Ø WARD COUNCILLOR:  
Cllr R Maidment (Neighbouring Ward)

Verbal Representations received:

Ø WARD COUNCILLOR:

Cllr K Rampton

Cllr P Broadhead

**RESOLVED that the application be granted Planning Permission in line with recommendation as set out in the report.**

For – 9                      Against – 2                      Abstain - 0

NOTE: Cllrs S McCormack and T Trent did not participate during the item as per their declarations of interest earlier.

Two Councillors did not vote due to technical difficulties.

The Chairman experienced technical difficulties during the main body of the item and therefore did not take any further involvement following this. Cllr D Kelsey was nominated as Chair of for the remainder of the item, as both the Chair and Vice-Chair had excluded themselves for the item.

160. Plot W (Land east of Bishops Close and south of Gallop Way), Purchase Road, Poole

(Talbot and Branksome Woods)

APP/19/00949/F

Temporary Use of part of Plot W for contractor car parking for a period of 15 months. Works to erect fencing and lighting.

Written Representations received:

Ø IN OBJECTION:

H Bateman

K Lingane

Mr and Mrs Odell

N Dobbs

S Ricketts

Ø IN SUPPORT:

M McFarland

Ø WARD COUNCILLOR:

None

Verbal Representations received:

Ø WARD COUNCILLOR:

Cllr K Rampton

Cllr P Broadhead



During the debate a Motion to grant Planning Permission was LOST.

**RESOLVED that: the application be refused Planning Permission due to environmental concerns and the development's effect on neighbours, such as: noise, light and disturbance, as per policies PP21 and PP32.**

For – 8          Against – 5          Abstain - 0

NOTE: Cllrs S McCormack and T Trent did not participate during the item as per their declarations of interest.

Cllr T Johnson requested that his vote against the resolution be recorded.

161. Former Winter Gardens site, Keystone House and 20, 20a and 20b Exeter Road Bournemouth BH2 5AR

(Bournemouth Central)

7-2020-1273-BB

Minor material amendment to remove conditions a and b of 7-2017-1273-AY, vary conditions c, 1 and 18 of 7-2017-1273-AY, and condition 2 of 7-2019-1273-BA, to include additional flats, reduction to basement structure and associated alterations to car park, changes to landscaping, redistribution of leisure space, reduction in size of the convenience store and office space, reduction in residential parking spaces and elevation changes. (Outline Planning Application for demolition of existing buildings & construction of a mixed use scheme; comprising residential apartments (use class C3) in buildings between 3 storeys & 15 storeys, leisure (use class D2), convenience retail (use class A1), restaurant/cafe (use class A3), mixed use restaurant/bar (use class A3/A4), offices (use class B1), associated servicing & loading areas, public car parking, private car parking, public open space, public realm enhancements & associated engineering works. Proposal affects a public right of way.)

Written Representations received:

Ø IN OBJECTION:

None

Ø IN SUPPORT:

P Lamb

R Mears

Ø WARD COUNCILLORS:

None

Verbal Representations received:

Ø WARD COUNCILLOR:

Cllr M Greene

**RESOLVED that the application be granted Planning Permission in line with the recommendation as set out in the report.**

For – 5            Against – 4            Abstain – 4

Note: Cllrs D Mellor and P Hall left the meeting before item 6f.

162.    Request for extension to complete Section106s

The Committee were asked to consider an update on the planning applications at Reid Steel, Reid Street, Christchurch & 88 Glenville Road, Christchurch and to give approval for extending the time for completing the s106 agreements.

App. No. 8/18/3532/OUT

App. No. 8/19/1282/FUL

**RESOLVED that:- an extension to complete the s106 agreement in respect of App. No. 8/18/3532/OUT until 31 March 2021 be approved and; a retrospective extension to the completion of the S106 agreement until 31 May 2020 to allow the issuing of the permission for App. No. 8/19/1282/FUL within that time limit also be approved.**

Voting: Unanimous.

163.    Protocol for Public Statements at Planning Committee

The Committee considered the Protocol for Public Statements at Planning Committee, with the recommendation being to agree as proposed and to make such amendments as was considered appropriate.

**RESOLVED that the Protocol for Public Statements at Planning Committee be agreed as proposed.**

Voting: Unanimous.

The meeting ended at 7.45 pm

CHAIRMAN

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## PLANNING COMMITTEE

### PROTOCOL FOR PUBLIC STATEMENTS AT MEETINGS

#### (ARTICLE 16: COVID-19 INTERIM DECISION MAKING ARRANGEMENTS)

This protocol makes provision for public statements to be taken into account in the decision making process at virtual meetings of the Planning Committee. It enables objectors and applicants/supporters to submit a written statement on planning applications for consideration at the meeting where they would normally submit a request to attend and speak at a physical meeting. These statements will be read out at the meeting on their behalf.

This protocol is separate from and is not intended to replicate or replace the procedure of submitting a written representation on a planning application to the Planning Offices during the consultation period.

1. Objectors and applicants/supporters, including Parish or Town Council representatives, who wish to provide a written statement to be read out on their behalf at the Planning Committee must submit this to Democratic Services by 12noon on the day before the meeting.
2. There will be a maximum of two statements from objectors and a maximum of two statements from applicants/supporters on each planning application considered by the Committee. Each statement may consist of up to 450 words.
3. Statements will be accepted on a first come, first served basis. Statements will not be accepted once the limit has been reached. Objectors, and applicants/supporters with similar views are encouraged to co-ordinate in advance in the production of statements.
4. Statements will be read aloud by the Democratic Services Officer once the Presenting Officer has completed their presentation on each planning application.
5. Ward Councillors who have referred an application to the Planning Committee for decision will be expected to attend and speak at the meeting wherever possible, to explain their reasons for the call in. Other Ward Councillors may also wish to attend and speak at the meeting.
6. Any Ward Councillor attending and speaking at the meeting must also submit a written version of what they intend to say to Democratic Services by 12noon on the day before the meeting. In the event of a Ward Councillor not being able to access the meeting at the appropriate time for any reason, this statement will be read out on their behalf to ensure their views can be taken into account. Statements may consist of up to 900 words.
7. Any member of the Planning Committee who has referred an application to the Committee for decision and who has a predetermined view on that application may speak as a Ward Councillor in accordance with the provisions in this protocol, but will not be able to participate in the discussion or vote as a member of the Committee.
8. Written statements should refer to planning related issues as these are the only matters the Committee can consider when making decisions on planning applications.

Statements must direct points to reinforcing or amplifying the planning representations already made to the Council in writing. Guidance on what constitutes planning considerations is included at the end of this document. Statements must not include derogatory or defamatory comments.

9. Anyone submitting a written statement who wishes to provide still photographs or illustrations (a maximum of five) to be displayed on screen while their statement is being read aloud must submit these to Democratic Services by 12noon TWO DAYS before the meeting.
10. Presentations other than those by the Presenting Officer(s) will not be facilitated at the meeting.
11. Any updates on planning applications to be considered by the Committee will be published by Democratic Services as soon as possible after 12noon on the day before the meeting.
12. In considering each application the Committee will normally consider contributions from people in the following order:
  - Presenting Officer(s)
  - Objectors
  - Applicant/Supporters
  - Ward Councillors (for the avoidance of doubt and for the purposes of this protocol, the term 'ward councillor' means a councillor who is not a member of the planning committee)
  - Questions and discussion by Members of the Planning Committee, which may include points of clarification from Officers, leading to a decision.
13. Exceptionally, in cases of significant major planning applications the Chairman of the Planning Committee may exercise discretion in respect of provisions within this protocol. Arrangements will be agreed in advance in consultation with Planning Services and Democratic Services.
14. Please note that virtual meetings of the Planning Committee are recorded for live and subsequent broadcast by the Council, and will be published on the Council's website for a minimum of six months after the meeting date. Agenda, reports and broadcasts can be accessed using the following link:  
<https://democracy.bcpCouncil.gov.uk/ieListMeetings.aspx?CId=290&Year=0>

For further information about public statements at Planning Committee please contact [democratic.services@bcpCouncil.gov.uk](mailto:democratic.services@bcpCouncil.gov.uk)

This Protocol has been adopted in accordance with the provisions of Article 16 of the Council's Constitution - Covid-19 Interim Decision Making Arrangements.  
A copy of the Council's Constitution can be accessed using the following link:  
<https://democracy.bcpCouncil.gov.uk/ieListDocuments.aspx?CId=151&MIId=4091&Ver=4&Info=1>

The National Planning Portal provides the following guidance on material planning considerations:

'A material consideration is a matter that should be taken into account in deciding a planning application or on an appeal against a planning decision. Material considerations can include (but are not limited to):

- Overlooking/loss of privacy
- Loss of light or overshadowing
- Parking
- Highway safety
- Traffic
- Noise
- Effect on listed building and conservation area
- Layout and density of building
- Design, appearance and materials
- Government policy
- Disabled persons' access
- Proposals in the Development Plan
- Previous planning decisions (including appeal decisions)
- Nature conservation

However, issues such as loss of view, or negative effect on the value of properties are not material considerations.'

[https://www.planningportal.co.uk/faqs/fag/4/what\\_are\\_material\\_considerations](https://www.planningportal.co.uk/faqs/fag/4/what_are_material_considerations)

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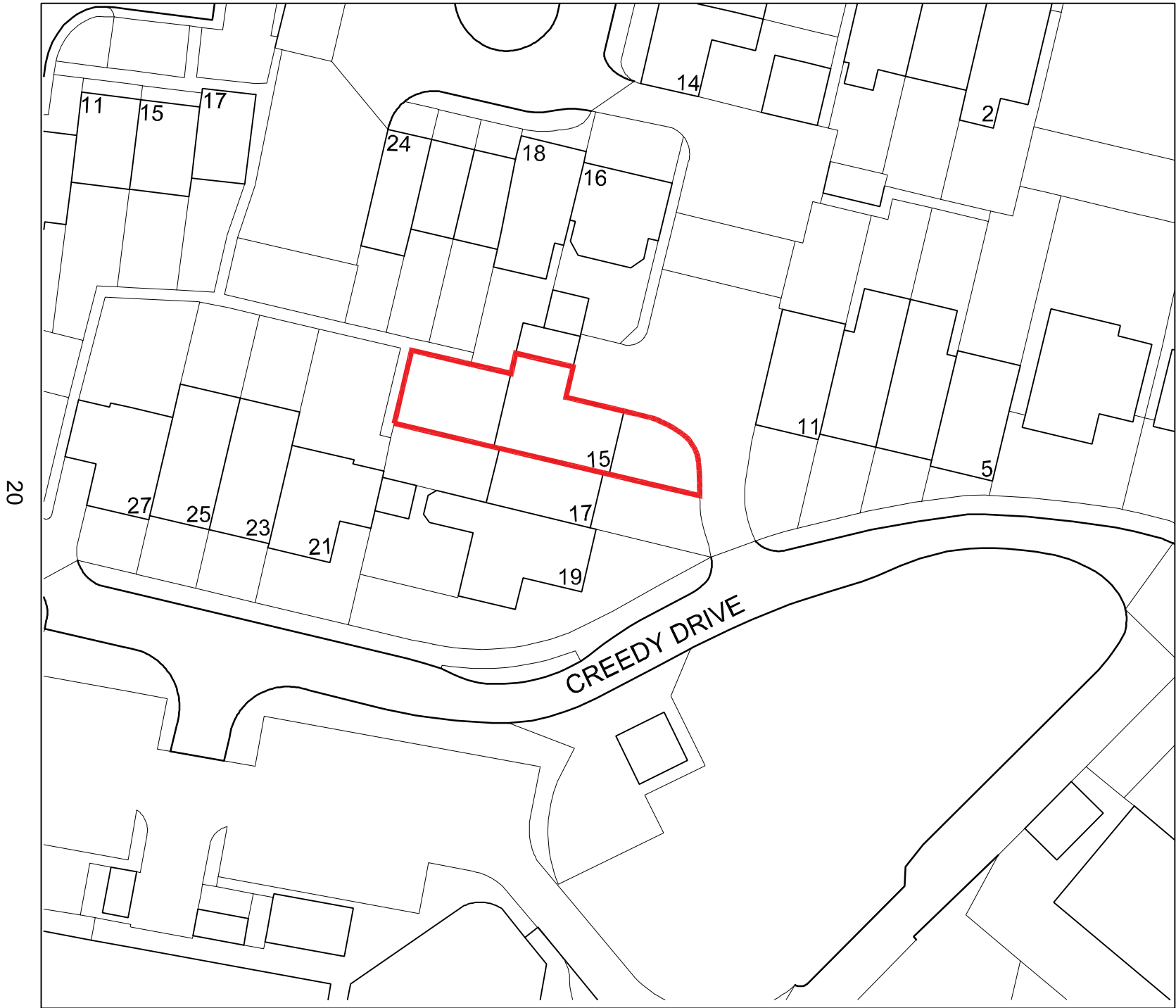


## Addendum Sheet Planning Committee – 11<sup>th</sup> June 2020

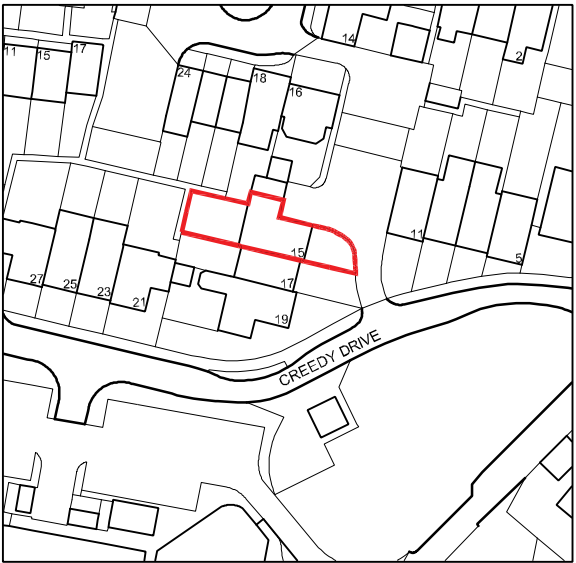
Agenda Items		
05	Planning Appeals	

PLANNING APPLICATIONS																							
Item No.	Application No.																						
01	10 Suffolk Road	<p>Replace paragraph 3 with:</p> <p>3. The applicant has provided the following information:</p> <table border="1"> <thead> <tr> <th></th><th>Existing</th><th>Proposed</th></tr> </thead> <tbody> <tr> <td>Use</td><td>Hostel</td><td>Flats</td></tr> <tr> <td>Number of Units</td><td>22</td><td>31</td></tr> <tr> <td>Car Parking Spaces</td><td>0</td><td>27</td></tr> <tr> <td>Cycle Parking Spaces</td><td>0</td><td>35</td></tr> <tr> <td>Maximum Height</td><td>8.3m</td><td>18m to Cambridge Road 16.3m to Suffolk Road</td></tr> <tr> <td>Minimum Main Elevation Distance from Boundary</td><td>1.5m</td><td>2.1m</td></tr> </tbody> </table> <p>Replace first sentence of paragraph 30 with:</p> <p><i>30. 11 Cambridge Road</i> – this property is located to the north east of the site sharing a common boundary and is a two storey to eaves plus mansard roof block of flats.</p>		Existing	Proposed	Use	Hostel	Flats	Number of Units	22	31	Car Parking Spaces	0	27	Cycle Parking Spaces	0	35	Maximum Height	8.3m	18m to Cambridge Road 16.3m to Suffolk Road	Minimum Main Elevation Distance from Boundary	1.5m	2.1m
	Existing	Proposed																					
Use	Hostel	Flats																					
Number of Units	22	31																					
Car Parking Spaces	0	27																					
Cycle Parking Spaces	0	35																					
Maximum Height	8.3m	18m to Cambridge Road 16.3m to Suffolk Road																					
Minimum Main Elevation Distance from Boundary	1.5m	2.1m																					
02	Goods Yard																						
03	81-83 Hankinson Road	Paragraph 6 under the heading “relevant planning applications and appeals” at bullet point 2 replace reference 7-2011-20654-B with 7-2010-24416.																					
04	134 High Street	<p>For <b>Condition 2</b> (Match Materials to the existing building), substitute the following:</p> <p>The materials and finishes to be employed on the external faces of the development, including that of the windows and dormers, hereby permitted shall be as specified on the submitted plans.</p> <p>Reason -</p> <p>To ensure a satisfactory visual relationship of the new development and that existing and in accordance with Policies PP27 and PP30 of the Poole Local Plan (November 2018).</p>																					
05	15 Creedy Drive	Accurate plans pack attached. (Initial plans on agenda included plans from a different scheme and omitted the exiting/proposed floor plans. Apologies for the error).																					

- NOTES:**
1. The contents of this drawing are copyright.
  2. Scaled drawing for Planning purposes only.
  3. Contractors must verify all dimensions and report any discrepancies before putting work in hand or making any shop drawings.
  4. Lower ground construction/ retaining structure to be structural engineers design.
  5. Electrical layouts to be agreed with client & added to drawing
  6. Drawings to be read in conjunction with specification.



**Block Plan (1:500)**  
Ordnance Survey, (c) Crown  
Copyright 2015. All rights reserved.  
Licence number 100022432



**Location Plan (1:1250)**  
Ordnance Survey, (c) Crown  
Copyright 2015. All rights reserved.  
Licence number 100022432

Rev	Date	Description	By	PM
<b>REVISIONS:</b>				

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr. Brown

**PROJECT & DRAWING TITLE:**

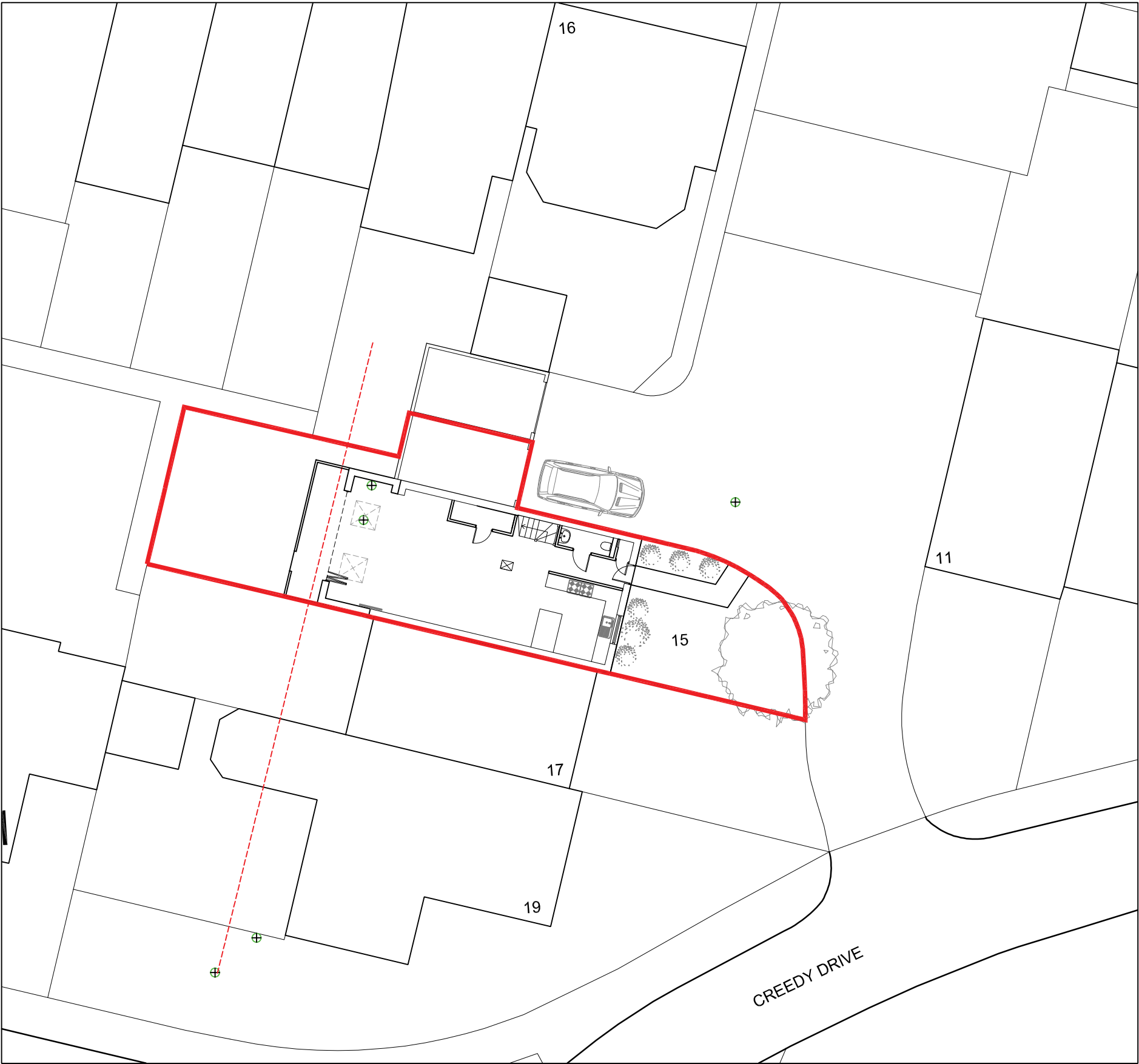
15 Creedy Drive, Christchurch  
Block and Location Plan

<b>Scale @</b> A3:12500, 1:1250	<b>Drawn by :</b> SK	
<b>Date :</b> 29/08/19	<b>Checked by :</b> MS	
<b>DRAWING No:</b> ASP.19.090.001		<b>REVISION:</b> 1

**ARCHITECTURE - PLANNING - PROJECT MANAGEMENT - CDM**

17a High Street, Christchurch,  
Dorset, BH23 1AB  
01202 472222  
[www.aspirearchitects.co.uk](http://www.aspirearchitects.co.uk)  
[info@aspirearchitects.co.uk](mailto:info@aspirearchitects.co.uk)  
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- NOTES:**
1. The contents of this drawing are copyright.
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  4. Lower ground construction/ retaining structure to be structural engineers design.
  5. Electrical layouts to be agreed with client & added to drawing
  6. Drawings to be read in conjunction with specification.

----- Drainage path

A	08.04.20	Minor amendment made to proposed balustrade	CW
Rev	Date	Description	By
REVISIONS:			

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Proposed Site Plan

Scale @ A3: 1:200	Drawn by : SR	
Date : 11/09/19	Checked by : MS	
DRAWING No:	REVISION:	
ASP.19.090.002	A	

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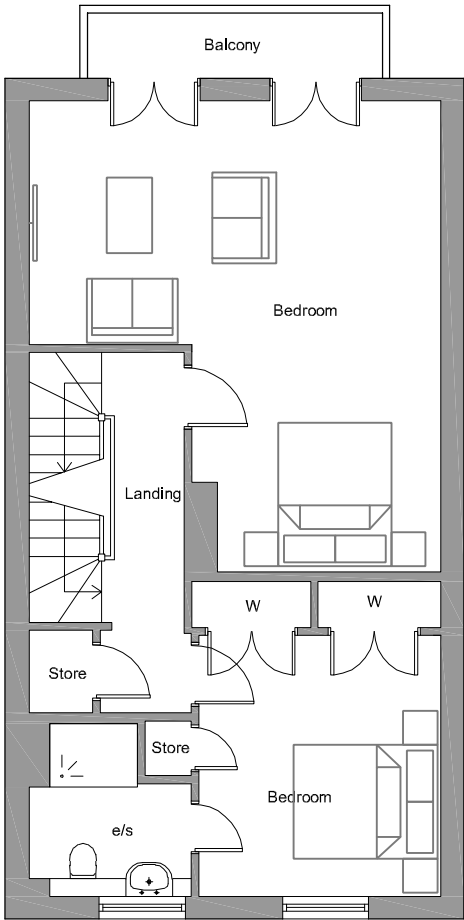


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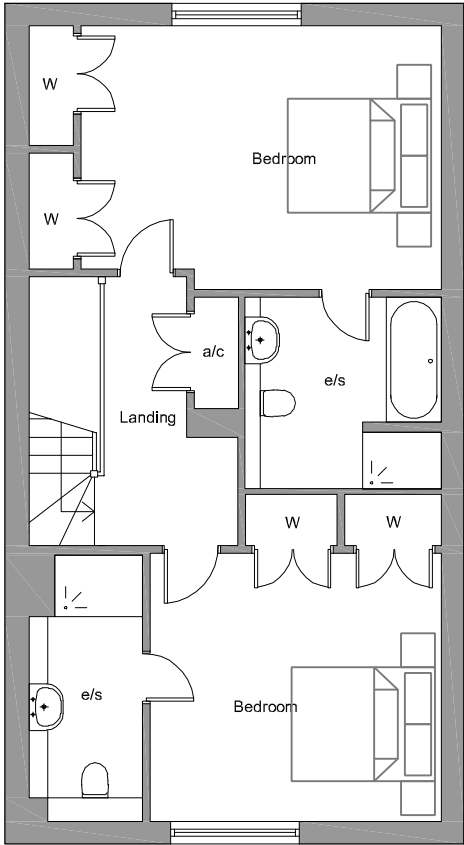
Proposed Site Plan 1:200  
0 20m



Ground Floor Plan 1:100



First Floor Plan 1:100



Second Floor Plan 1:100

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  4. Lower ground construction/ retaining structure to be structural engineers design.
  5. Electrical layouts to be agreed with client & added to drawing
  6. Drawings to be read in conjunction with specification.

Rev	Date	Description	By	PM
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**REVISIONS:**

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

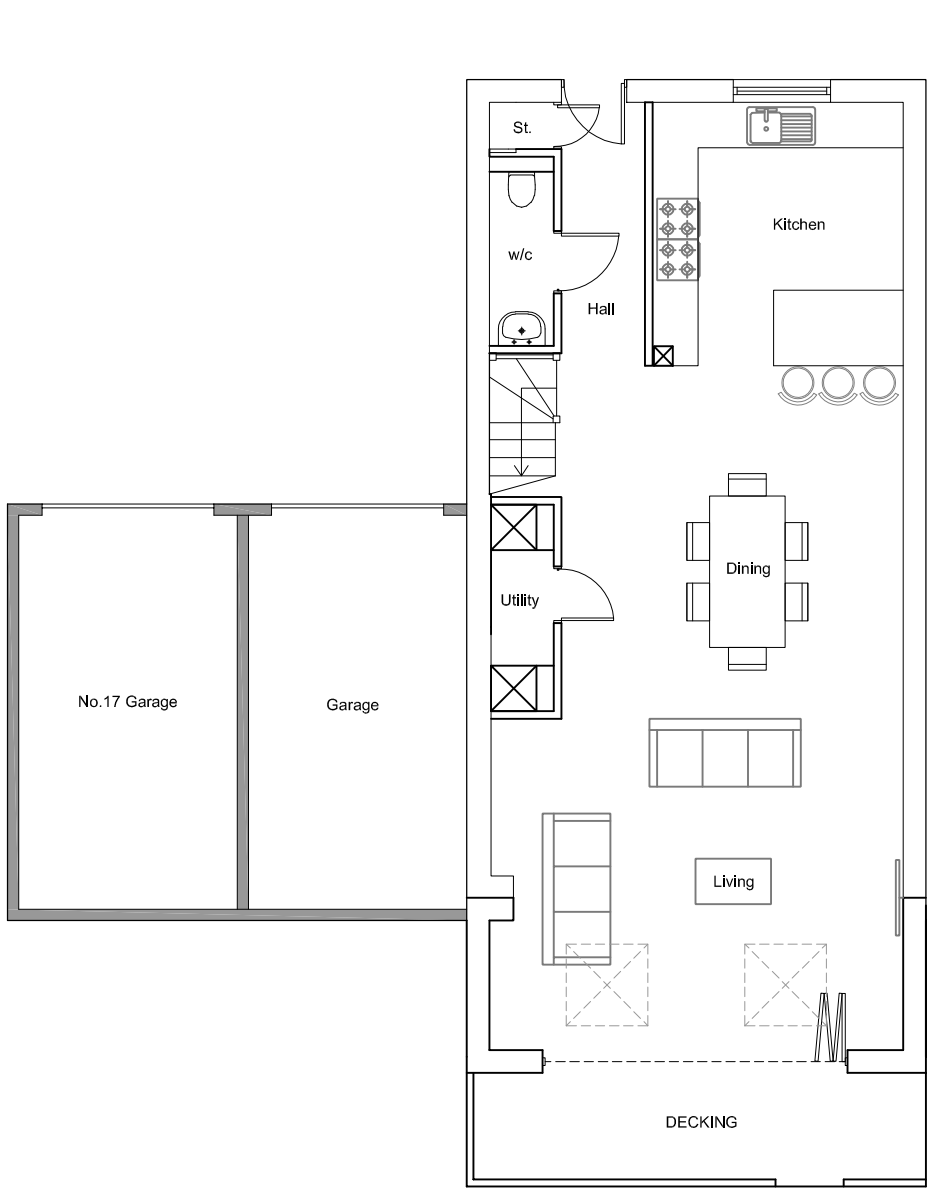
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Existing Plans

Scale @ A3: 1:100	Drawn by: SW	
Date: 28.05.19	Checked by: MS	
DRAWING No: ASP.19.090.003	REVISION:	

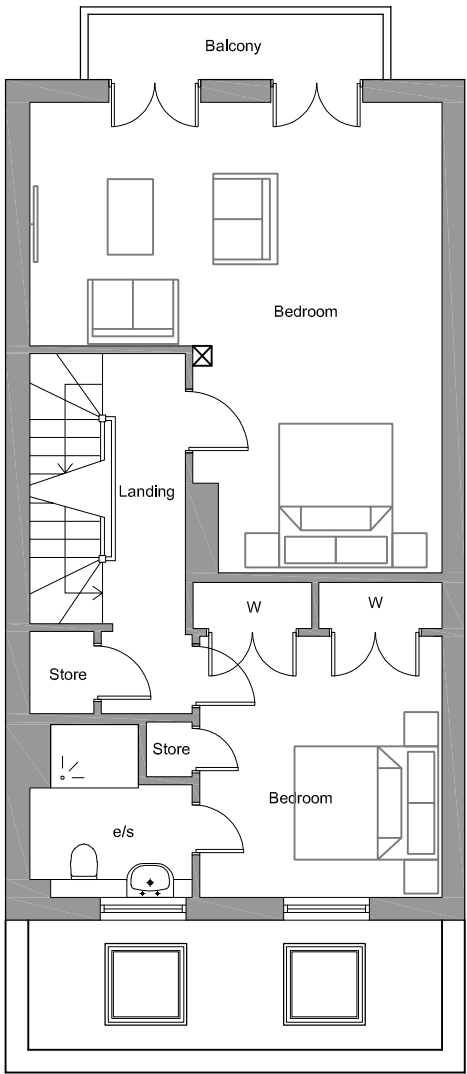
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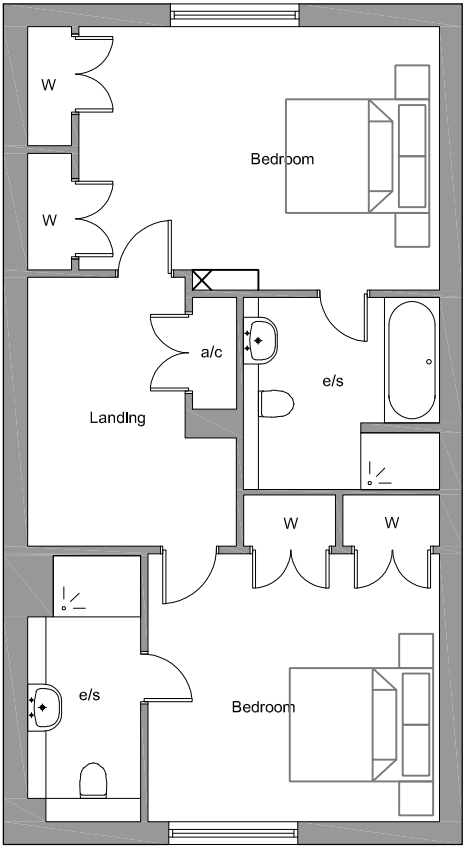




Ground Floor Plan 1:100



First Floor Plan 1:100



Second Floor Plan 1:100

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A	08.04.20	Minor amendment made to proposed balastrade	CW
Rev	Date	Description	By
REVISIONS:			

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Proposed Plans

Scale @ A3: 1:100	Drawn by: SR	
Date: 10/10/19	Checked by: MS	
DRAWING No: ASP.19.090.100		REVISION: A

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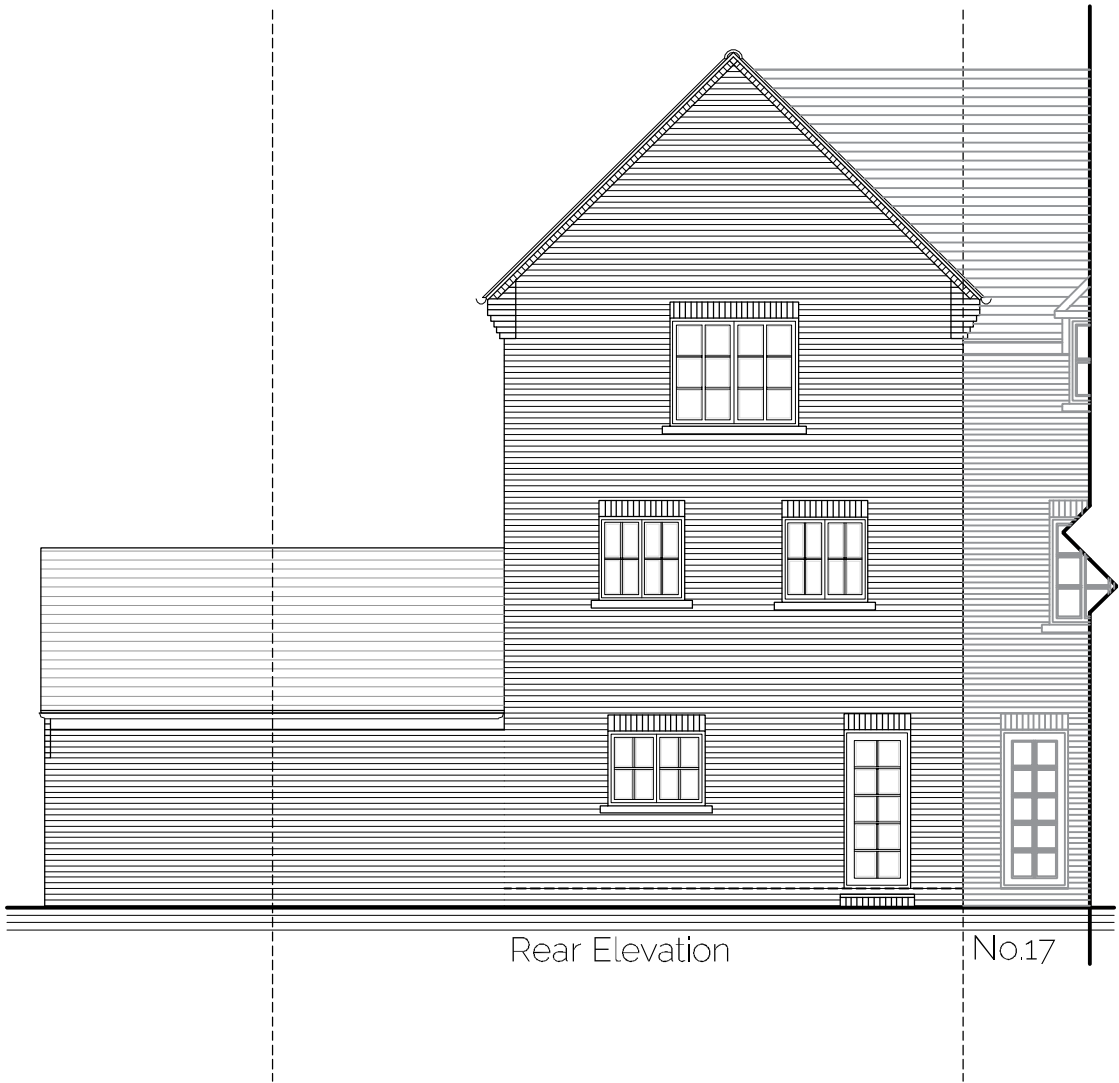
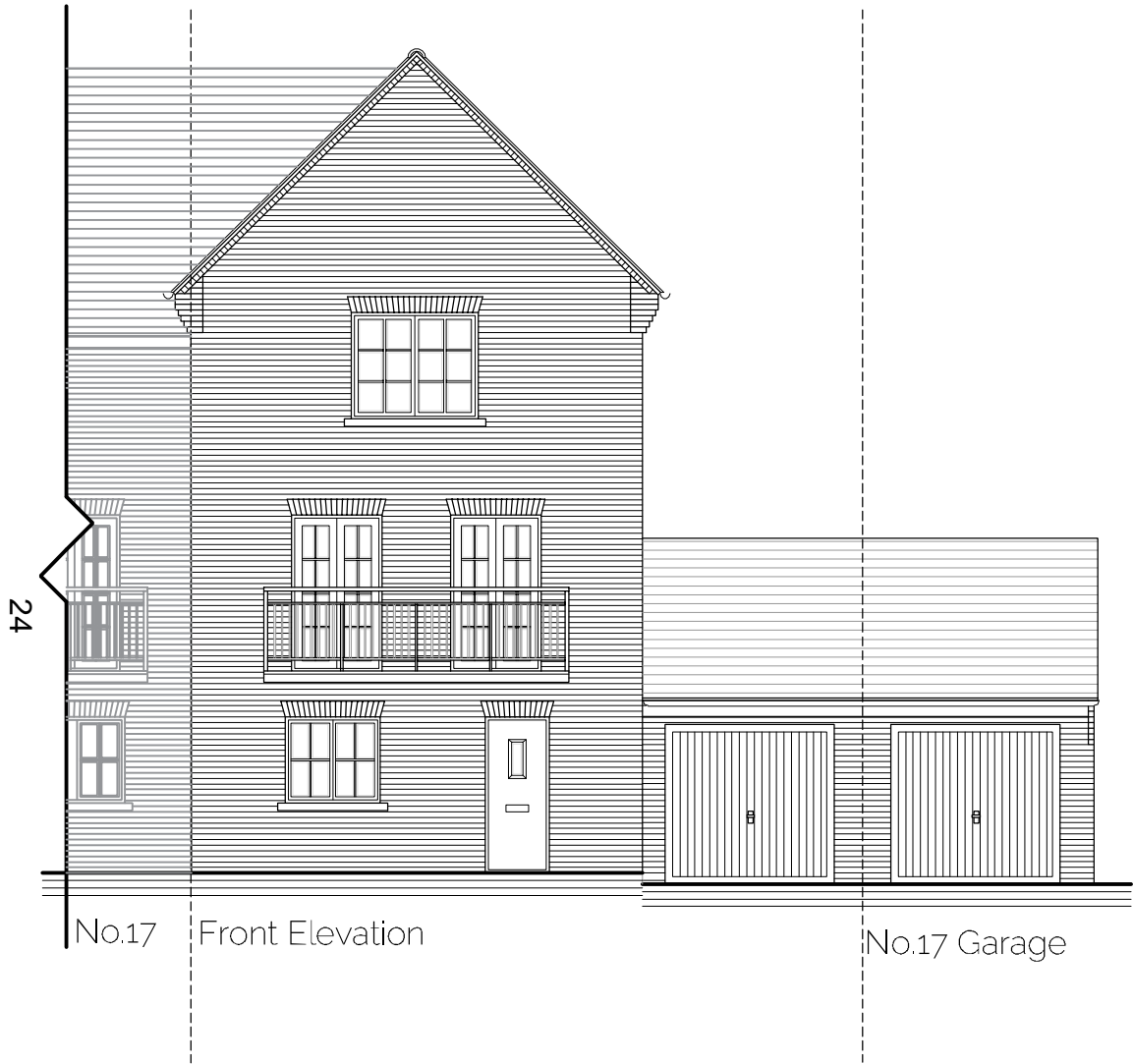
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A	05.04.20	Amendments made to No.17	Cw/
Rev	Date	Description	By PM

**REVISIONS:**

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

<b>PROJECT &amp; DRAWING TITLE:</b>		
15 Creedy Drive, Christchurch Existing Elevations		
Scale @ A3:1:100 Date: 20.05.19	Drawn by: SH Checked by: MS	
DRAWING No: ASP.19.090.004	REVISION: A	

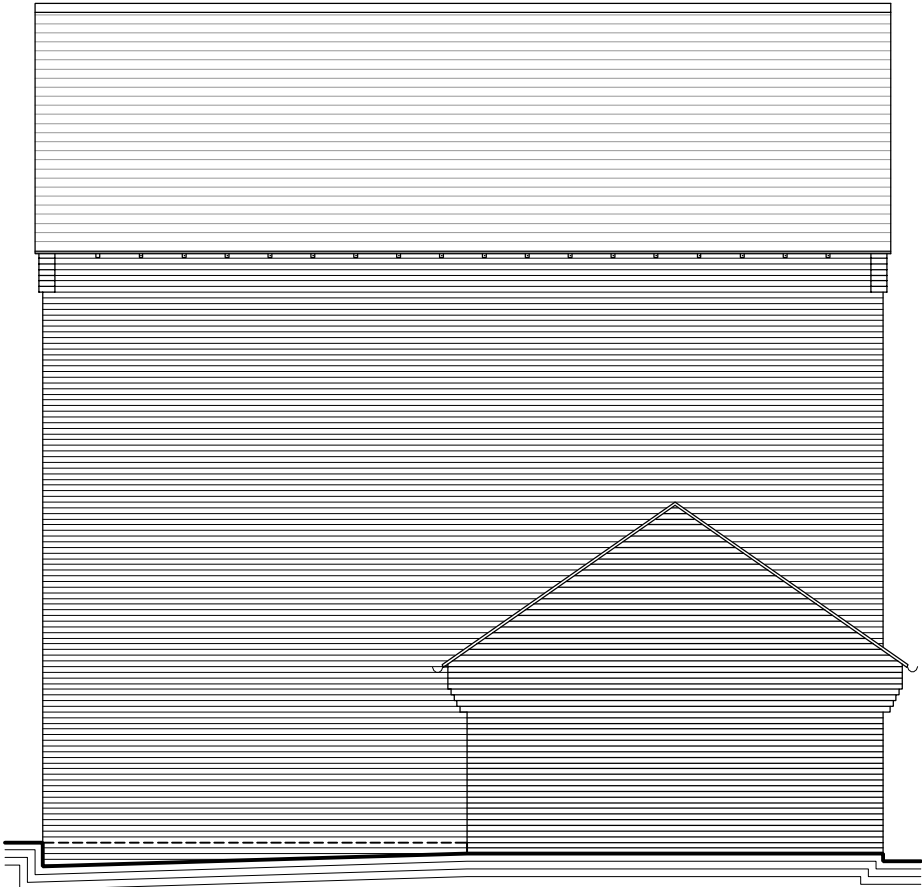
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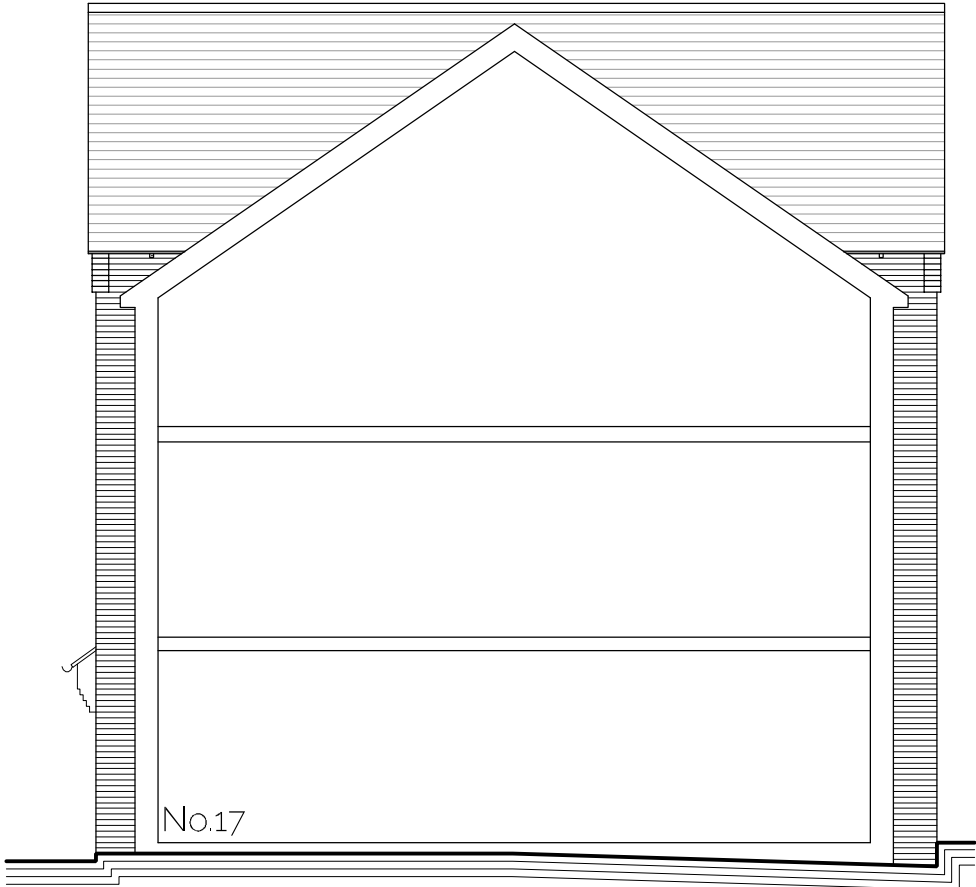


Existing Elevations 1:100  
0 10m

25



Side Elevation



Side Elevation

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A	05.04.20	Additional elevation added for clarity	CW
Rev	Date	Description	By PM

**REVISIONS:**

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Existing Elevations

Scale @ A3:1:100	Drawn by : SH	
Date : 05.05.19	Checked by : MS	
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Existing Elevations 1:100







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Rev	Date	Description	By PM

**REVISIONS:**

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Proposed Elevation

Scale @ A3:1:100	Drawn by : SR	REVISION: A
Date : 10.05/19	Checked by : MS	
DRAWING No: ASP.19.090.200		

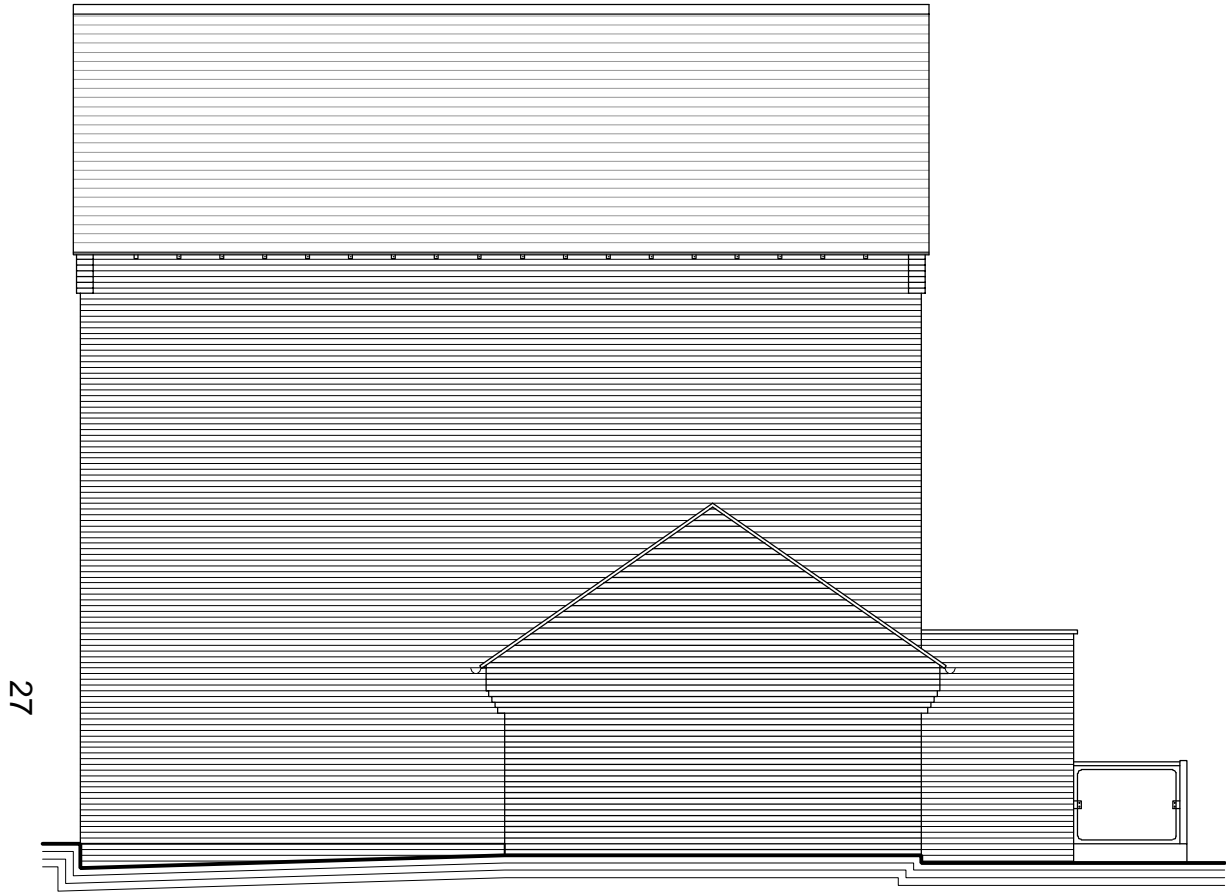
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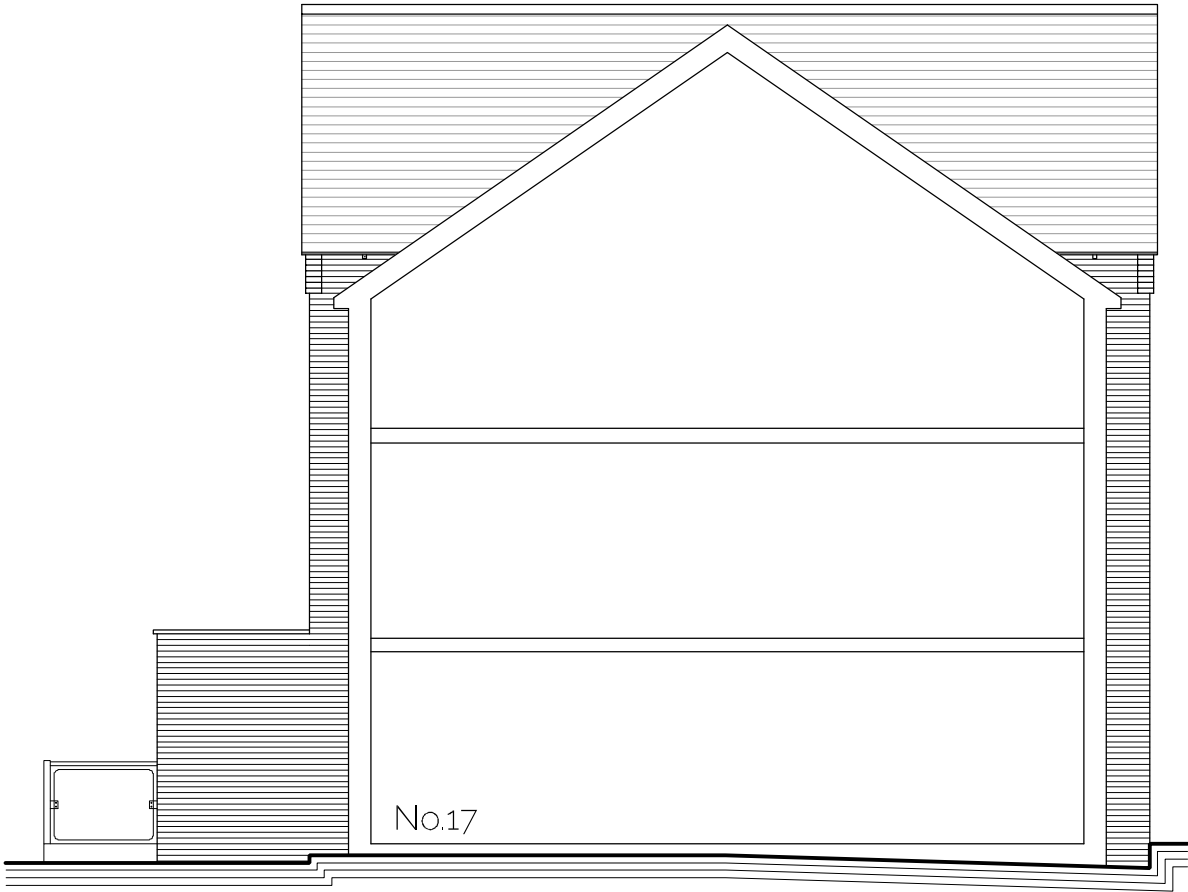


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Side Elevation



Side Elevation

A	08.04.20	Additional elevation added for clarity	CW	
Rev	Date	Description	By	PM

**REVISIONS:**

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Proposed Elevation

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Proposed Elevations 1:100  
0

10m

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# Planning Committee

Application Address	10 Suffolk Road
Proposal	Outline submission for the erection of a 3 to 6 storey building of 31 flats with basement parking and formation of vehicular access
Application Number	7-2019-6638-J
Applicant	Bournemouth Churches Housing Association
Agent	Ken Parke Planning Consultants
Date Application Valid	20 February 2019
Decision Due Date	21 May 2019
Extension of Time date (if applicable)	TBC
Ward	Central - Pre May 2019
Report Status	Public
Meeting Date	11 June 2020
<b>Recommendation</b>	<b>Grant in accordance with the recommendation below</b>
Reason for Referral to Planning Committee	More than 10 letters of objection received (pre April 2019) and call in from Councillor Mike Greene on the grounds of mass, height and bulk; highways issues and negative effect on the amenity of local residents.
Case Officer	Charles Raven

## Description of Development

- Outline planning permission is sought for the erection of a 3 to 6 storey building of 31 flats with basement parking and the formation of vehicular access. The applicant has confirmed that the matters to consider are:  
  
Access, Appearance, Layout and Scale.
- Landscaping is reserved for subsequent consideration.

3. The applicant has provided the following information:

	Existing	Proposed
Use	Hostel	Flats
Number of Units	22	31
Car Parking Spaces	0	27
Cycle Parking Spaces	0	35
Maximum Height		
Minimum Distance from Boundary		

4. A number of amendments were provided during the consideration of the application. These included a reduction in the height and bulk of the built form adjacent to neighbouring properties, changes to the access and parking layout, as well as the addition of a number of balconies. The footprint remained unchanged.
5. This application was submitted to the former Bournemouth Borough Council and fell under the previous scheme of delegation. The delay in bringing to the Planning Committee was due to a continuing dialogue on issues relating to tree protection, highways and impact on neighbours.

### **Key Issues**

6. The main considerations involved with this application are:

Impact on character and appearance of the area;  
Impact on residential amenity;  
Impact on highway safety/parking;  
Impact on trees;  
Impact on heathlands;  
Affordable housing provision.

7. These points will be discussed as well as other material considerations at para 20 to 49 below.

### **Planning Policies**

8. **Bournemouth Local Plan: Core Strategy (2012)**

Policy CS1 – Presumption in Favour of Sustainable Development  
Policy CS4 – Surface Water Flooding  
Policy CS6 – Delivering Sustainable Communities  
Policy CS16 – Parking Standards  
Policy CS18 – Increasing Opportunities for Cycling and Walking  
Policy CS21 – Housing Distribution Across Bournemouth  
Policy CS33 – Heathland  
Policy CS41 – Quality Design

9. **Bournemouth District Wide Local Plan (2002)**

Policy 4.25 - Landscaping

10. **Bournemouth Local Plan: Town Centre Area Action Plan (2013)**

Policy D4 – Design Quality  
Policy U2 – Housing

11. **Supplementary Planning Documents:**

Affordable Housing DPD – Policy AH1  
Dorset Heathlands Planning Framework – SPD  
Residential Development: A Design Guide 2008 - PGN  
Sustainable Urban Drainage Systems (SUDS) – PGN  
Bournemouth Parking SPD  
Bournemouth Town Centre Development Design Guide SPD

12. **The National Planning Policy Framework (2019)**

Paragraph 11 sets out the presumption in favour of sustainable development. Development plan proposals that accord with the development plan should be approved without delay. Where the development plan is absent, silent or relevant policies are out-of-date then permission should be granted unless any adverse impacts of approval would significantly and demonstrably outweigh the benefits when assessed against the NPPF.

**Relevant Planning Applications and Appeals:**

13. March 2002 - Alterations to side elevation and formation of a separate unit - GRANTED
14. June 1991 - Erection of a 3 storey hostel for the homeless and formation of parking spaces – GRANTED

**Representations**

15. Site notices were posted in the vicinity of the site on 25/02/2019 with an expiry date for consultation of 22/03/2019.
16. 22 representations have been received, 21 raising objection; and 1 comment from Bournemouth Police. The issues raised comprise the following:-
- Height
  - Scale
  - Out of keeping
  - Highway safety
  - Loss of privacy
  - Overlooking
  - Overbearing
  - Loss of light
17. Impact on property value or loss of a view are not material planning considerations

**Consultations**

18. Urban Design – no objection following revisions

Highways – no objection following revisions

Trees – no objection following revisions

Recycling & Waste – a private collection will be required

## **Constraints**

19. Tree Preservation Order on neighbouring site.

## **Planning Assessment**

### **Site and Surroundings**

20. The site occupies a corner plot with frontages to Suffolk Road and Cambridge Road. Most buildings in the immediate vicinity range from two to five storeys, with taller buildings up to seven storeys on the opposite side of the dual carriageway. Cambridge Road slopes up from north to south with the sites to the north being around 1-2m below the level of the application site. Properties to the south are consequently set at a higher level than the application site.
21. The existing building is operated by the Bournemouth Churches Housing Association (BCHA) and currently in use as a hostel which originally provided supported housing for ex-offenders, but which now helps people out of homelessness. The building has 22 rooms arranged as bedsitting rooms and cluster units with communal facilities on the ground floor.
22. The proposals entail the demolition of the existing two/three storey building and the erection of a three to six storey block of 31 flats with two levels of basement car parking. The accommodation would be arranged as 14 studio flats, 15 one bedroom flats, and 2 two bedroom flats.
23. The development has been subject to pre-application advice which originally proposed a development of up to 8 storeys in height with greater site coverage.

## **Key Issues**

### **Impact on character and appearance of the area**

24. The site occupies a corner plot with frontages to Suffolk Road and Cambridge Road. Most buildings in the immediate vicinity range from two to four storeys. The proposal entails the demolition of an existing two/three storey building in use as a hostel for the homeless and the erection of an up to six storey block of 31 flats with basement parking. By reason of the sloping nature of the site, 5 floors will provide living accommodation. As this is a corner plot, it is considered appropriate for the development to rise higher than its immediate neighbours. The submitted street scene drawings demonstrate the changes in levels and relationships with neighbouring buildings and show that the proposed maximum height is commensurate with the full height of Rivera Court on the opposite side of Suffolk Road (albeit the upper floor is largely recessed and only includes the lift and stairwell). However, the small increase in height over the existing surrounding built form is not considered harmful or out of character.





Cambridge Road



Suffolk Road

25. Officers have negotiated a reduction in the scale of the built form adjacent to both 11 Cambridge Road and 8 Suffolk Road, so the proposed building height next to these properties is more reflective of the heights of those respective buildings, stepping up to address the corner location.
26. The street elevations benefit from generous fenestration, depth and interest and are generally well ordered. The building turns the corner well and the main entrance to Suffolk Road is well articulated. The design of the building is contemporary and there are not any issues with this approach. The buildings immediately surrounding the site, for the most part, are finished with mansard roofs. Whilst design will always be a subjective issue, mansard roofs are not considered a positive design feature and the local buildings are very much of their time.
27. The development is considered to make efficient use of the site, particularly with basement parking removing unsightly surface parking areas. The parking area is on two levels, the first accessed straight off Cambridge Road. The lower level is access via car lifts. A second lift was introduced following discussion with officers to ensure continuity should one fail.
28. Whilst it may not be apparent from the current plans, the scheme has been significantly scaled back in terms of built form from the 8 storey development proposed at pre-application stage and, as stated above, from the original submission. Comments from your Urban Design Officer have been incorporated into the revised design. The scale and design of the development have been fully considered and are considered appropriate and would not have an adverse impact on the character or appearance of the area.

#### Impact on Residential Amenity

29. *8 Suffolk Road* – this property is located to the north east of the site sharing a side boundary. The building is a two storey to eaves plus mansard roof block of flats. The existing building at 10 Suffolk Road is designed so there are not any windows serving main

habitable rooms on the elevation facing 8 Suffolk Road. In addition, the main bulk of building is concentrated away from this property with only two storey elements in close proximity to the boundary. As discussed previously, the scale of built form of the proposed building has been reduced in the area closest to the boundary with this property so is three storeys in height at this point. The stepped nature of the development also means that the upper floors would be largely unviewable from the closest windows on all levels from this property. Proposed windows are also angled away so there are not any direct window to window relationships. Balconies that could potentially overlook are fitted with privacy screens. Given the changes to the scheme, the impact on the occupiers of this property is considered acceptable.

30. *11 Cambridge Road* – this property is located to the south east of the site sharing a common boundary and is a two storey to eaves plus mansard roof block of flats. The existing building at 10 Suffolk Road is designed so there are not any windows serving main habitable rooms on the elevation facing 11 Cambridge Road. In addition, the second floor of No.10 is stepped away from the edge of the ground and first floors so has minimal impact on No.11. No.11 is a later development and was designed taking into account the layout, design, and siting of No.10. Therefore, No.11 has windows serving main habitable rooms on its flank elevation facing the proposal site. In addition to the negotiated reduction in height of built form adjacent to this property, potential issues of overlooking have been mitigated through the repositioning of windows to ensure no direct window to window relationships with main habitable rooms. Remaining windows would be secondary and fitted with obscure glazing. Privacy screens to the three balconies on this elevation are also provided. Upper floor terraces would not be afforded any direct views into windows serving this property. Given the stepped nature of the proposal and the reduction in height of immediate adjacent built form, it is considered that the development would not result in an overbearing impact or a significant loss of direct sunlight which would be detrimental to the living conditions of the occupiers of this property. Given the changes to the scheme, the impact on the occupiers of this property is considered acceptable.
31. *Future Residents* – following discussions with officers, additional balconies have been added where appropriate, meaning 21 of the units will have access to a balcony. Whilst not formally adopted, all of the units meet or exceed the minimum standards contained within the nationally described space standard.

#### Impact on Highway Safety/Parking

32. The development has been assessed by your Highways Officer who has negotiated a number of revisions to enable support of the scheme.

#### *Access*

33. A new access to basement car park is proposed from Cambridge Road. The existing access from Suffolk Road is proposed to be closed. The new access is close to the junction with Suffolk Road, but turning manoeuvres already occur at this point, due to vehicles accessing the on-street parking bays. However, the loss of on-street car parking spaces should be identified on a plan together with an assessment of the line of sight/visibility splay to the south, in accordance with Manual for Streets. It is likely that no on-street parking will be possible south of the proposed access. A modification to the Traffic Regulation Order for Cambridge Road will also be required, at the cost of the applicant.

#### *Car parking provision*

34. The site is within Zone 1 of the Parking SPD, which estimates that the proposed development will generate a demand for 41 allocated parking spaces or 22 unallocated

spaces. 23 spaces are proposed following revisions, so they will need to be unallocated. The applicant has confirmed the spaces to be unallocated and this will also be conditioned.

#### *Car parking layout*

35. Car parking is proposed in two basement levels, named the upper and lower basement. The parking spaces are of the correct dimensions, a 500mm offset is required adjacent to internal walls, so that doors on both sides of the vehicle can be opened and has now been provided.
36. The upper basement is proposed to be accessed via a 1:10 ramp with acceptable headroom, whereas the lower basement is proposed to be served by a car lift. Vehicles can only enter and leave the lower basement via the proposed car lift. This did not provide sufficient resilience if the lift is out of order. Therefore, a second lift has now been provided which addresses this concern.

#### *Car park information system*

37. The submitted plans indicate a car parking information screen adjacent to the upper basement. This is welcomed but a display screen will also be required adjacent to the car lifts so that drivers can turn into the lifts if the upper basement is full. The display should indicate the availability of standard and disabled spaces.

#### *Visitor car parking*

38. The submitted plans indicate that visitors arriving by car will be able to use a buzzer/intercom system to access the basement, this is acceptable and can be conditioned.

#### *Cycle parking provision*

39. 35 cycle parking spaces are proposed in two separate cycle stores, one on the ground floor and the second in the upper basement. This is an acceptable level of provision. Revisions to the layout and access have addressed concerns previously raised.
40. Your Highways Officer does not raise any objections subject to conditions.

### Impact on Trees

41. The protected Corsican Pine and Western Red Cedar on the adjacent site are fine quality trees of high visual amenity value and their retention without damage is essential. Your Tree Officers have been in detailed discussions with the applicants consultant arboriculturalist. Following the provision of additional justification, Officers have assessed the amount of tree root harm that could occur and have found this to be limited. There are therefore no objections to the development subject to full compliance with the submitted arboricultural method statement at all times during the development phases.

### Sustainable Homes

42. Policy CS2 of the Core Strategy requires at least 10 percent of the energy to be used in developments of more than 10 dwellings to come from decentralised and renewable or low carbon sources. Details have not been provided at this stage, although the plans show an area for PV Panels on the roof of the block. A condition will ensure compliance with the policy.

### Heathland Mitigation

43. The site is within 5km of a designated Dorset Heathlands SPA (Special Protection Area) and Ramsar Site, and part of the Dorset Heaths candidate SAC (Special Area of Conservation) which covers the whole of Bournemouth. As such, the determination of any application for an additional dwelling(s) resulting in increased population and domestic animals should be undertaken with regard to the requirements of the Habitat Regulations 1994. It is considered that an appropriate assessment could not clearly demonstrate that there would not be an adverse effect on the integrity of the sites, particularly its effect upon bird and reptile habitats within the SSSI.
44. Therefore, as of 17<sup>th</sup> January 2007 all applications received for additional residential accommodation within the borough is subject to a financial contribution towards mitigation measures (SMM) towards the designated sites. A capital contribution is therefore required and in this instance is £8,339, plus a £417 administration fee. A signed legal agreement has been drafted to provide this contribution.

### Affordable Housing

45. All applications proposing residential development in excess of 10 units net will be subject to the Council's adopted affordable housing policy. The affordable housing DPD sets out an approach to achieving contributions towards the delivery of affordable housing in the borough. Policy AH1 contained within DPD requires all residential development to contribute towards meeting the Borough target of 40% affordable housing. When considering residential development the Council will seek a 40% contribution except where it is proven to not be financially viable. The DPD was revised in November 2011 and sets out in greater detail how the DPD will be implemented as well as including an indicative contribution table which applicants can agree to rather than submit viability information. In this instance, the applicant is a registered provider of social housing and will retain ownership of the units. In order to meet the requirements of the adopted affordable housing policy, a condition is proposed which ensures that at least 40% of the units are made available as affordable housing as defined by the NPPF. The applicant has agreed to the condition.

### Community Infrastructure Levy

46. As the site is located within the Town Centre Area Action Plan boundary, residential development is zero rated for CIL so a contribution is not required.

### Summary

47. It is considered that:

The development makes more efficient use of an underutilised site;  
The scale and design of the development are appropriate;  
The impact on neighbouring occupiers has been successfully mitigated;  
The level of parking provision conforms to the adopted Parking SPD;  
The development would not have an adverse impact on highway safety;  
There would not be a harmful impact on protected trees;  
Heathland mitigation and affordable housing provision are policy compliant.

## **Planning Balance**

48. It is acknowledged that the tallest part of the proposed building is higher than its immediate neighbours, however, taking the wider context into consideration, as well as the corner location and revisions negotiated, the height as proposed would be acceptable. The impact on neighbouring occupiers has been fully explored and mitigated where necessary. The development provides needed living accommodation within a town centre and sustainable location.
49. Therefore, having considered the appropriate development plan policy and other material considerations, including the NPPF, it is considered that subject to compliance with the conditions attached to this permission, the development would be in accordance with the Development Plan, would not materially harm the character or appearance of the area or the amenities of neighbouring and proposed occupiers and would be acceptable in terms of traffic safety and convenience. The Development Plan Policies considered in reaching this recommendation are set out above.

## **Recommendation**

50. **GRANT permission with the following conditions, which are subject to alteration/addition by the Head of Planning and Building Control provided any alteration/addition does not go to the core of the decision and the completion of a Section 106 agreement with the following terms:**

### **Section 106 terms**

Heathland Mitigation (SAMM): £8,339 plus £417 admin  
Transport Contribution: £800

### **Conditions**

#### **1. Development to be carried out in accordance with plans as listed**

The development hereby permitted shall be carried out in accordance with the following approved plans: 1426-51C, 1426-56B, 1426-57E, 1426-58B, 1426-59C, 1426-60B, 1426-61C, 1426-62D, 1426-63D, 1426-64D, 1426-65B, 1426-66C, 1426-67D, 1426-68D, 1426-69B, 1426-70C, 1426-84.

Reason: For the avoidance of doubt and in the interests of proper planning.

#### **2. On site working hours (inc demolition) restricted when implementing permission**

All on-site working, including demolition and deliveries to and from the site, associated with the implementation of this planning permission shall only be carried out between the hours of 8 a.m. and 6 p.m. Monday - Friday, 8 a.m. and 1 p.m. Saturday and not at all on Sunday, Public and Bank Holidays unless otherwise agreed in writing with the Local Planning Authority.

Reason: To safeguard the amenities of occupiers of adjoining and nearby properties and in accordance with Policies CS14 and CS38 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **3. Method statement to be submitted to include operatives' car parking, noise reduction measures, storage of materials**

No site clearance or development work shall commence until there has been submitted to and approved in writing by the Local Planning Authority a Method Statement that includes the following measures:

- a) parking arrangements for operatives and construction vehicles working on-site;
- b) noise reduction measures [including times of piling operations]; and the
- c) details and siting of equipment, machinery and surplus materials on the site.

The parking arrangements for operatives and construction vehicles shall be implemented prior to development commencing and the development shall be carried out in accordance with the approved details.

Reason: To safeguard the amenities of occupiers of adjoining and nearby properties and in the interest of highway safety in accordance with Policies CS38, CS41 and CS14 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **4. Construction Management Plan**

Prior to the commencement of the development, a detailed Construction Management Plan shall be prepared and submitted for written approval of the Local Planning Authority in conjunction with the Local Highway Authority. The Construction Management Plan shall include details of safe access to the site for deliveries, loading and unloading of plant and materials and wheel cleansing of vehicles prior to egress from the site onto the public highway. The approved Construction Management Plan shall be implemented and complied with upon commencement of the development and the obligations within the Construction Management Plan shall be adhered to throughout the construction phase of the development.

Reason: In the interests of highway safety and good amenity, in accordance with Policy CS41 of the Bournemouth Local Plan Core Strategy (October 2012).

### **5. Sustainable Homes**

Prior to the commencement of development, an energy and sustainability appraisal shall be submitted in writing and the written approval to the details obtained from the Local Planning Authority, demonstrating that at least 10% of the energy to be used in the development will come from a decentralised and renewable or low carbon source. The agreed recommendations of the appraisal shall be implemented in full.

Reason: In order to reduce the carbon footprint of the development in accordance with Policy CS2 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **6. Prior Approval of Materials**

Details/samples of the materials to be used on the external surfaces of the proposed development shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of any superstructure works on site. Development shall be carried out in accordance with the approved details.

Reason: To ensure a satisfactory visual relationship between the existing and the new development in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **7. Surface Water Drainage (SUDS Implementation)**

Before the commencement of development, unless otherwise agreed in writing by the Local Planning Authority, a scheme for the whole site providing for the disposal of surface water run-off and incorporating sustainable urban drainage systems (SUDS), shall be submitted to and

approved in writing by the Local Planning Authority. The drainage works shall be completed in accordance with the approved details prior to occupation of the development or in accordance with a timetable to be agreed in writing by the Local Planning Authority. The scheme shall include the following as appropriate:

- a) A scaled plan indicating the extent, position and type of all proposed hard surfacing (e.g. drives, parking areas, paths, patios) and roofed areas.
- b) Details of the method of disposal for all areas including means of treatment or interception for potentially polluted run off.
- c) Scaled drawings including cross section, to illustrate the construction method and materials to be used for the hard surfacing (sample materials and literature demonstrating permeability may be required).

Reason: To provide satisfactory drainage for the development in accordance with Policy CS4 of the Bournemouth Local Plan: Core Strategy (October 2012) and in order to achieve the objectives set out in the Local Planning Authority's Planning Guidance Note on Sustainable Urban Drainage Systems.

### **8. Drainage Hard surfaced areas**

Any new or replacement hard surfaced area(s) shall either be made of porous materials, or provision shall be made to direct run- off water from the hard surface to a permeable or porous area or surface within the curtilage of the property.

Reason: To provide satisfactory drainage for the development in accordance with Policy CS4 of the Bournemouth Local Plan: Core Strategy (October 2012) and in order to achieve the objectives set out in the Local Planning Authority's Planning Guidance Note on Sustainable Urban Drainage Systems.

Note: Further guidance in this regard is contained in the Department for Communities and Local Government publication entitled "Guidance on the Permeable Surfacing of Front Gardens" (September 2008).

### **9. Affordable Housing**

At least 40% of the approved units on the site shall be provided as affordable housing. The development hereby approved shall not be occupied until a scheme for the provision of the affordable housing has been submitted to and approved in writing by the Local Planning Authority. The affordable housing shall be provided in accordance with the approved scheme and shall meet the definition of affordable housing in Annex 2 of the NPPF or any future guidance that replaces it, and shall be retained as such thereafter. The scheme shall include:

- i. The tenure of the affordable housing provision
- ii. The arrangements for the transfer of the affordable housing to an affordable housing provider or the management of the affordable housing (if no RSL is involved) ;
- iii. The arrangements to ensure that such provision is affordable for both first and subsequent occupiers of the affordable housing; and
- iv. The occupancy criteria to be used for determining the identity of occupiers of the affordable housing and the means by which such occupancy criteria shall be enforced.

Reason: In order to deliver mixed, balanced and sustainable communities in meeting the lack of affordable housing provision in the borough and in accordance with the adopted Affordable Housing Development Plan Document (2009).

### **10. Scheme for external pipework**

Prior to the installation of any external pipe work and/or flues to the building(s), a scheme for external pipe work and flues shall be submitted to and approved in writing by the Council. Works shall be carried out strictly in accordance with the approved scheme and unless shown on the

approved elevation drawings any pipe work (with the exception of rainwater down-pipes) shall be internal to the building.

Reason: In the interests of the visual amenities of the locality and in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **11. Turning/ Parking**

Before occupation of the development, the parking and turning areas shall be laid out in accordance with the approved plans using a bound material and shall be permanently maintained and retained thereafter.

Reason: In the interests of highway safety and in accordance with Policies CS14 and CS16 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **12. Pedestrian Inter-visibility Splays**

Before occupation of the development, pedestrian inter-visibility splays of 2m x 2m shall be provided on both sides of all vehicular accesses. No fence, wall or other obstruction to visibility over 0.6m in height above ground level shall be erected within the area of the splay at any time.

Reason: In the interests of highway safety and in accordance with Policy CS14 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **13. Unallocated parking**

All car parking spaces shown on the approved plans shall be made available for any resident of the development and those persons visiting residents of the development and shall remain unallocated to any specific resident or residence for the lifetime of the development.

Reason: In the interests of highway safety and quality design in accordance with Policies CS16 and CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **14. Car lifts, car park information system and visitor access**

Prior to the occupation of the development hereby approved, unless agreed in writing by the Local Planning Authority, details of the operation and maintenance of the car lifts, car park information system and visitor access system shall be submitted to and approved in writing by the Local Planning Authority. The approved details shall be implemented on site and shall be retained and maintained thereafter unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of highway safety and quality design in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **15. Cycle Store for Flats**

The cycle stores shown on the approved plans shall be completed prior to the occupation of any of part of the development granted by this permission. The cycle stores shall have lockable doors and keys should be made available to all residents. The cycle stores shall have internal and external illumination and adequate lighting shall be provided along the route to the stores. The cycle stores and associated lighting shall thereafter be retained, maintained and kept available for the residents of the development.

Reason: To promote alternative modes of transport and in the interests of amenity in accordance with Policies CS18 and CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).



## **16. Implementation of the approved Arboricultural Method Statement**

The tree protection measures as detailed in the arboricultural method statement dated 10 December 2019 and prepared by Treecall Consulting Ltd shall be implemented in full and in accordance with the approved timetable and maintained and supervised until completion of the development.

Reason: To ensure that trees and other vegetation to be retained are not damaged during construction works and to accord with Policy 4.25 of the Bournemouth District Wide Local Plan (February 2002).

## **17. Windows in NE Elevation to be Glazed with Obscure Glass**

The proposed window(s) in the North East facing (rear) elevation of the building as shown on the approved drawings, shall be glazed with obscure glass to a level equivalent to Pilkington Level 3 or above (or the nearest equivalent standard) and shall be permanently retained as such unless otherwise agreed in writing by the Local Planning Authority.

Reason: To prevent undue overlooking of the adjoining residential property and in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

## **18. Privacy Screens**

The proposed balconies on the SE side and NE rear elevations as shown on the approved drawings shall be provided with 1.8m high privacy screens in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. The privacy screens shall be sited on the elevations facing the neighbouring buildings before the development hereby approved is first occupied in full or in part and thereafter maintained and retained for that purpose.

Reason: To protect the privacy of occupiers of the adjoining dwelling in accordance with saved Policy 6.10 of the District Wide Local Plan and Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

## **19. Provision of Refuse Bin Store**

The bin stores hereby approved shall be constructed in accordance with the approved details prior to the occupation of the proposed development and shall be retained and maintained for that use thereafter.

Reason: To preserve the visual amenities of the locality in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

## **20. Provision of a Refuse Management Plan**

The development hereby permitted shall not be occupied until a Refuse Management Plan has been submitted to and approved in writing by the Local Planning Authority. The plan shall include: details of the management company to be set up; the employment of a private contractor to collect the refuse; measures to be taken if no private contractor is available at any time in the future (such as the employment of a person or persons to ensure bins are wheeled to the collection point); and that bins will not be stored in the open or at the collection point apart from on the day of collection. The refuse management plan shall be carried out in accordance with the approved details, unless otherwise approved in writing by the Local Planning Authority.

Reason: To ensure that the proposed development includes a long-term management plan for the collection of refuse in the interests of visual and residential amenities, and to accord with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

## **21. Boundary /Subdivision Treatment**

Within 3 months of the date of commencement of the development, unless otherwise agreed in writing by the Local Planning Authority, details of boundary treatment and/or subdivision shall be submitted to and approved in writing by the Local Planning Authority. Details shall include a plan showing: the positions, height, design, and materials. The approved boundary treatment scheme shall be implemented in full prior to occupation or use of the development commencing and permanently retained and maintained unless otherwise agreed in writing by the Local Planning Authority.

Reason: In the interests of amenity and privacy and in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

INFORMATIVE NOTE: The applicant is advised that there should be no storage of any equipment, machinery or materials on the footway/highway this includes verges and/or shrub borders or beneath the crown spread of Council owned trees.

INFORMATIVE NOTE: The applicant is advised that in order to avoid contravention of highways legislation, provision shall be made in the design of the access/drive to ensure that no surface water or loose material drains/spills directly from the site onto the highway.

INFORMATIVE NOTE: The applicant is advised that notwithstanding this consent, Section 184 of the Highways Act 1980 requires the proper construction of vehicle crossings over kerbed footways, verges or other highway land. Normally this work will be undertaken at the expense of the applicant by the Highway Authority although on occasions there might be instances where the applicant under supervision can undertake this work. The applicant must contact the Service Director, Technical Services, Town Hall Annexe, St. Stephens Road, Bournemouth, BH2 6EA to initiate the procedure.

INFORMATIVE NOTE: The applicant is advised that a contribution towards heathland mitigation measures is required and will be secured by entering into an Agreement under Section 106 of the Town and Country Planning Act 1990 in compliance with Policy CS33, as well as the adopted Dorset Heathlands Planning Framework SPD



**Location Plan**  
Scale 1:1250



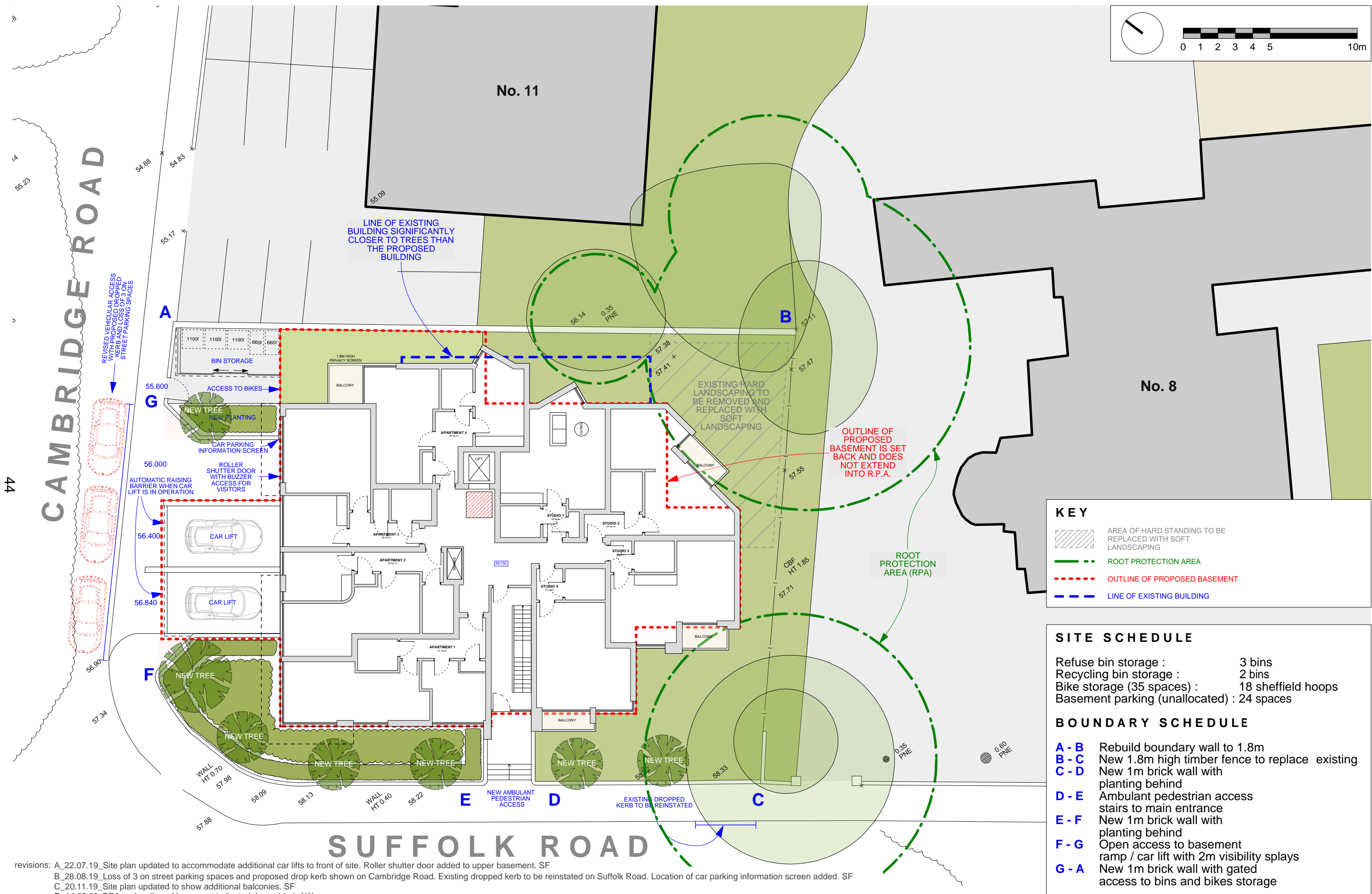
**Block Plan**  
Scale 1:500

--- Existing outline to be demolished  
 Proposed outline



revisions: A\_22.07.19\_Block plan updated to show additional car lifts to front of site. SF  
 B\_20.11.19\_Outline of proposed on block plan updated to show additional balconies. SF  
 C\_11.05.20\_Outline of proposed on block plan updated to show omitted balconies . HVG

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## MATERIALS KEY



Render painted white



Brick cladding



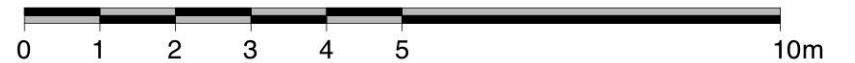
Aluminium framed doors,  
windows and capping



Timber doors and louvers



Obscure glazing



revisions: A\_22.07.19\_Elevation updated to show reduction of apartment 16 on the third floor and parapet roof heights aligned. SF  
B\_20.11.19\_Louvers omitted from front entrance to allow space for signage. Balconies added to studio flats 3,7 and 11. SF





MATERIALS KEY

Render painted white

Brick cladding

Aluminium framed doors, windows and capping

Timber doors and louvers

OG

Obscure glazing

012345

10m


revisions: A 22.07.19 Elevation updated to show reduction of apartment 12 on the second floor. Roller shutter added to upper basement and front boundary updated to accommodate additional car lifts. SF  
B 20.11.19 Balconies added to apartments 4 and 8 and studio flat 14. SF  
C 27.01.20 Studio 14 Balcony adjusted. RH

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# MATERIALS KEY

	Render painted white		Infinitem brick cladding		Aluminium framed doors, windows and capping		Timber doors and louvers		OG	Obscure glazing	
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revisions: A 22.07.19 Elevation updated to show reduction of apartment 16 on the third floor and parapet roof heights aligned. Lift and stair exterior walls on forth floor aligned. SF  
B 20.11.19 Balconies added to studio flats 1,2,3,5,6,7,9,10 and 11. SF  
C 23.01.19 Balconie Removed from Studio 6 & 10, Visibility Screen added to Studio 7 & 11, Following Planning Officers comments\_RH  
D\_11.05.20 Elevation updated to show omitted balconies. HVG





## MATERIALS KEY



Render painted white



Infinitum brick cladding



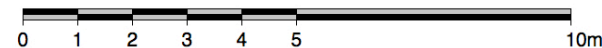
Aluminium framed doors,  
windows and capping



Timber doors and louvers



OG Obscure glazing

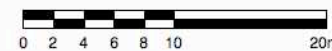


revisions: A 22.07.19 Elevation updated to show reduction of apartment 12 on the second floor and 16 on the third floor. Parapet roof heights aligned. SF  
B 26.11.19 Balconies added to studio flats 1,2,5,6,9,10 and 14 and apartments 4 and 5. Windows also added to kitchen and bathroom in studio flat 14. SF  
C 27.01.20 Studio 14 Balcony adjusted. RH  
D 11.05.20 Elevation updated to show omitted balconies. HVG





Indicative Street Scene ( **Suffolk Road** )



revisions: A\_22.07.19\_Streetscene updated to show reduction of apartment 16 on the third floor and parapet roof heights agligned . SF  
B\_20.11.19\_Streetscene updated to show additional balconies . SF



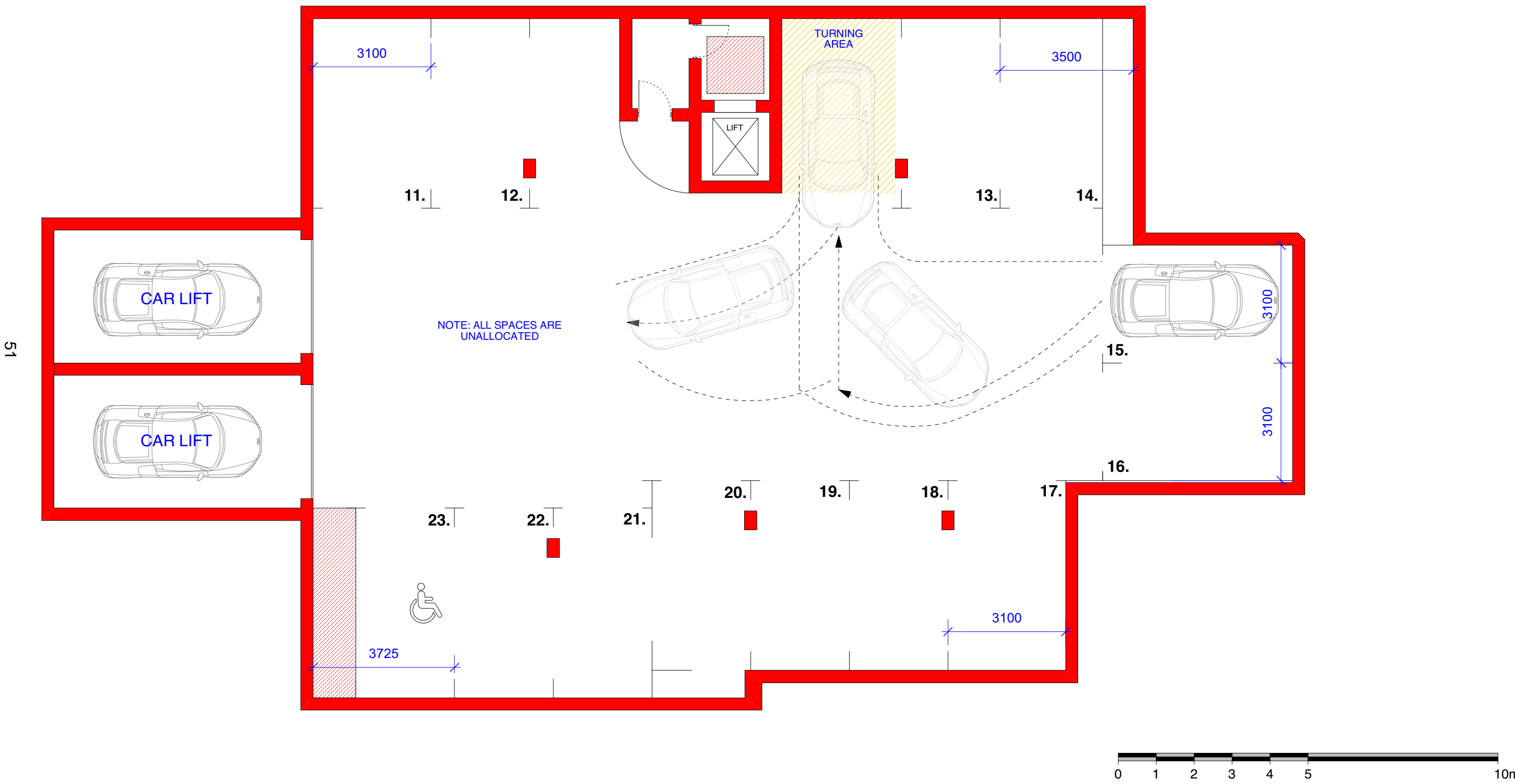
Indicative Street Scene ( Cambridge Road )



revisions: A 22.07.19 Streetscene updated to show reduction of apartment 12 on the second floor. Roller shutter added to upper basement and front boundary updated to accommodate additional car lifts. SF  
 B 20.11.19 Streetscene updated to show additional balconies. SF  
 C 27.01.20 Studio 14 Balcony adjusted. RH

Note: Do not scale from this drawing. The contractor is to check all dimensions on site. This drawing is copyright and remains the property of David James Architects & Partners Ltd. Unauthorised use for any purpose is prohibited.

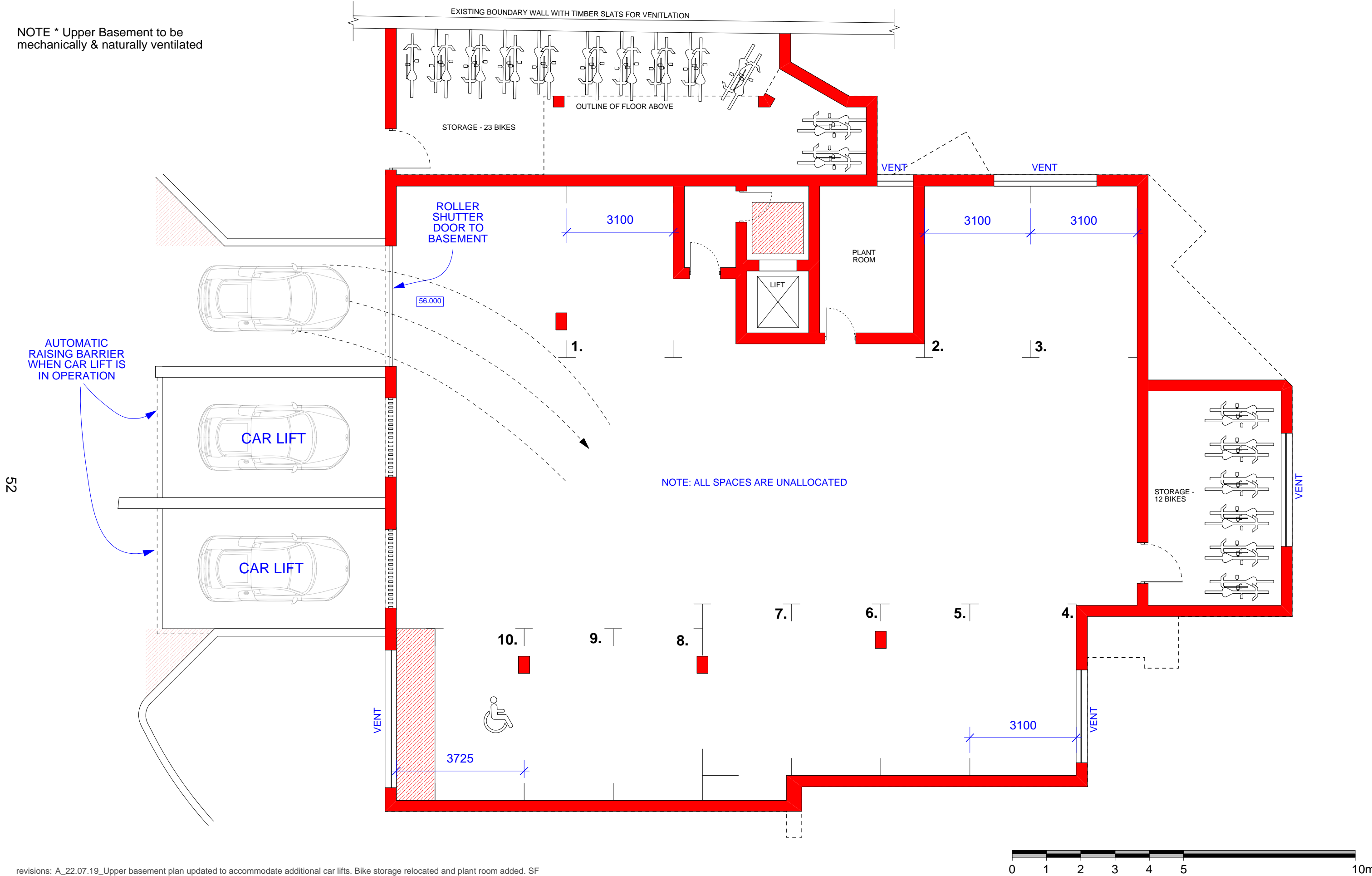
NOTE \* Lower Basement to be mechanically ventilated



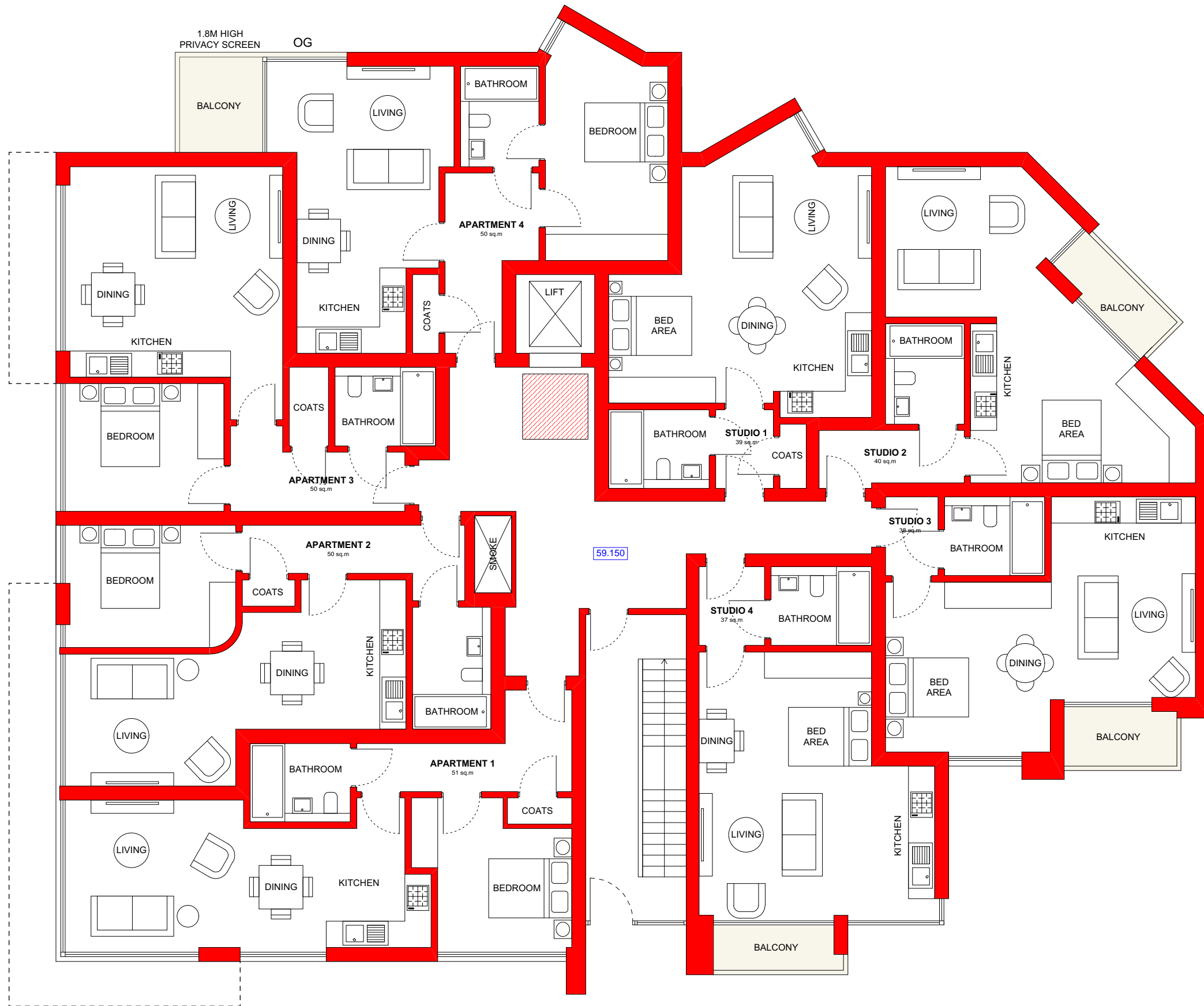
revisions: A\_22.07.19\_Lower basement plan updated to accommodate additional car lifts. SF  
B\_28.08.19\_Turning area has been marked with yellow hatching. Car parking layout amended to allow for extra space required next to internal walls. SF

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NOTE \* Upper Basement to be mechanically & naturally ventilated

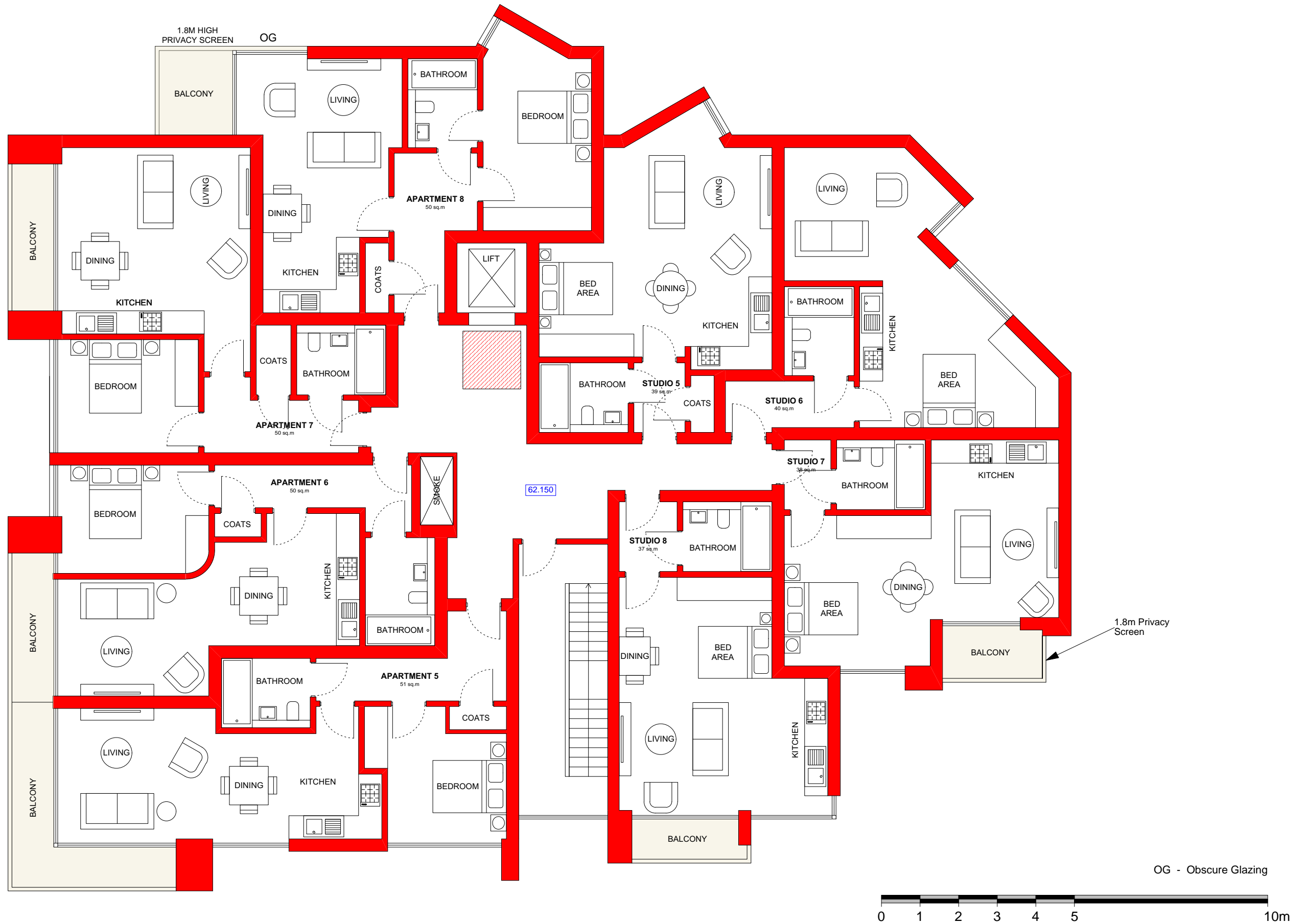


revisions: A\_22.07.19\_Upper basement plan updated to accommodate additional car lifts. Bike storage relocated and plant room added. SF  
B\_28.08.19\_Car parking layout amended to allow for extra space required next to internal walls. Door widths of cycle stores increased to meet regulations. SF  
C\_11.05.20\_Outline of proposed ground floor plan updated to show omitted balconies . HVG



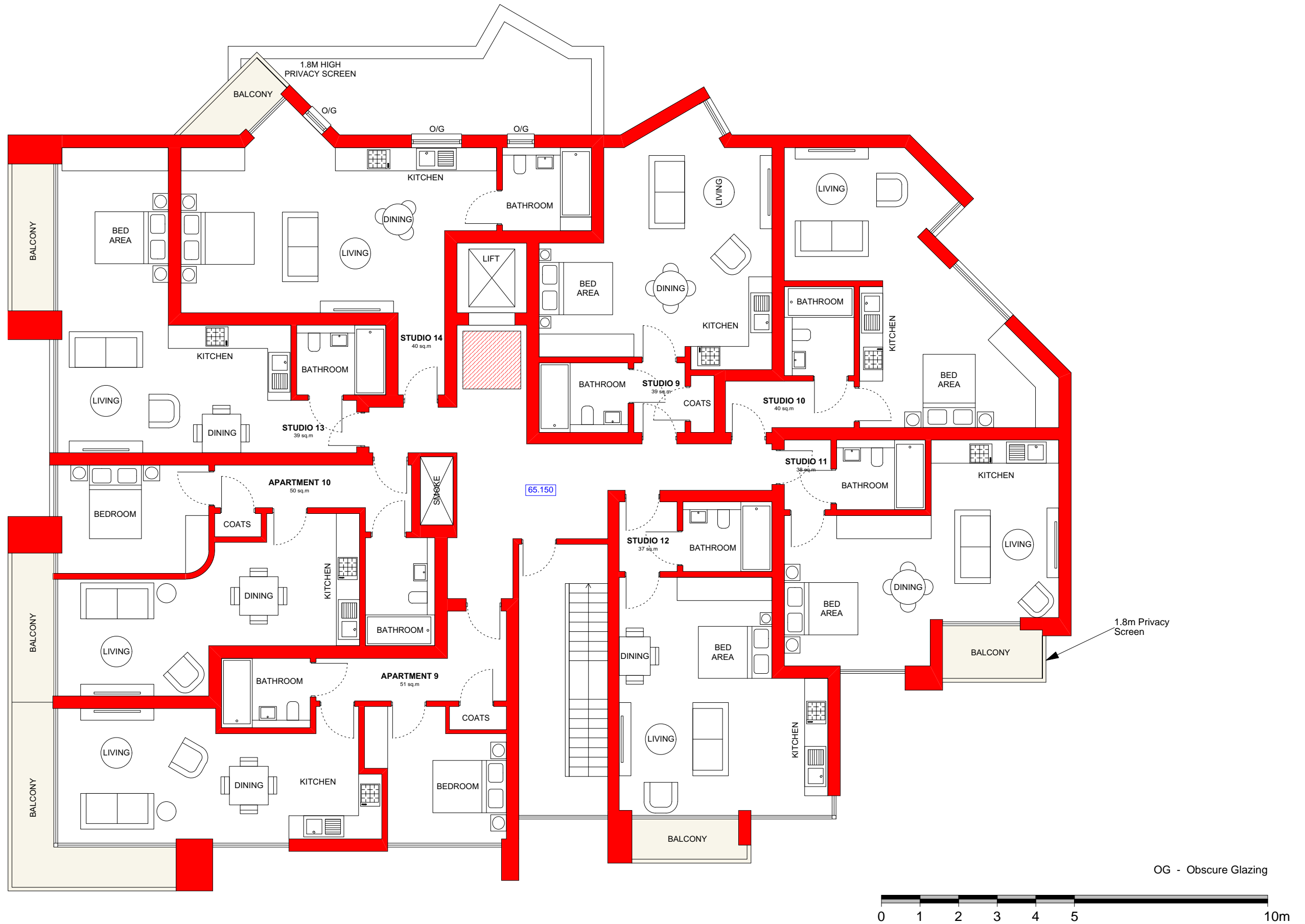
revisions: A\_20.11.19\_Balconies added to studio flats 1,2 and 3 and apartment 4. SF  
 B\_11.05.20\_Floor plan updated to show omitted balconies to Studio 1 . HVG

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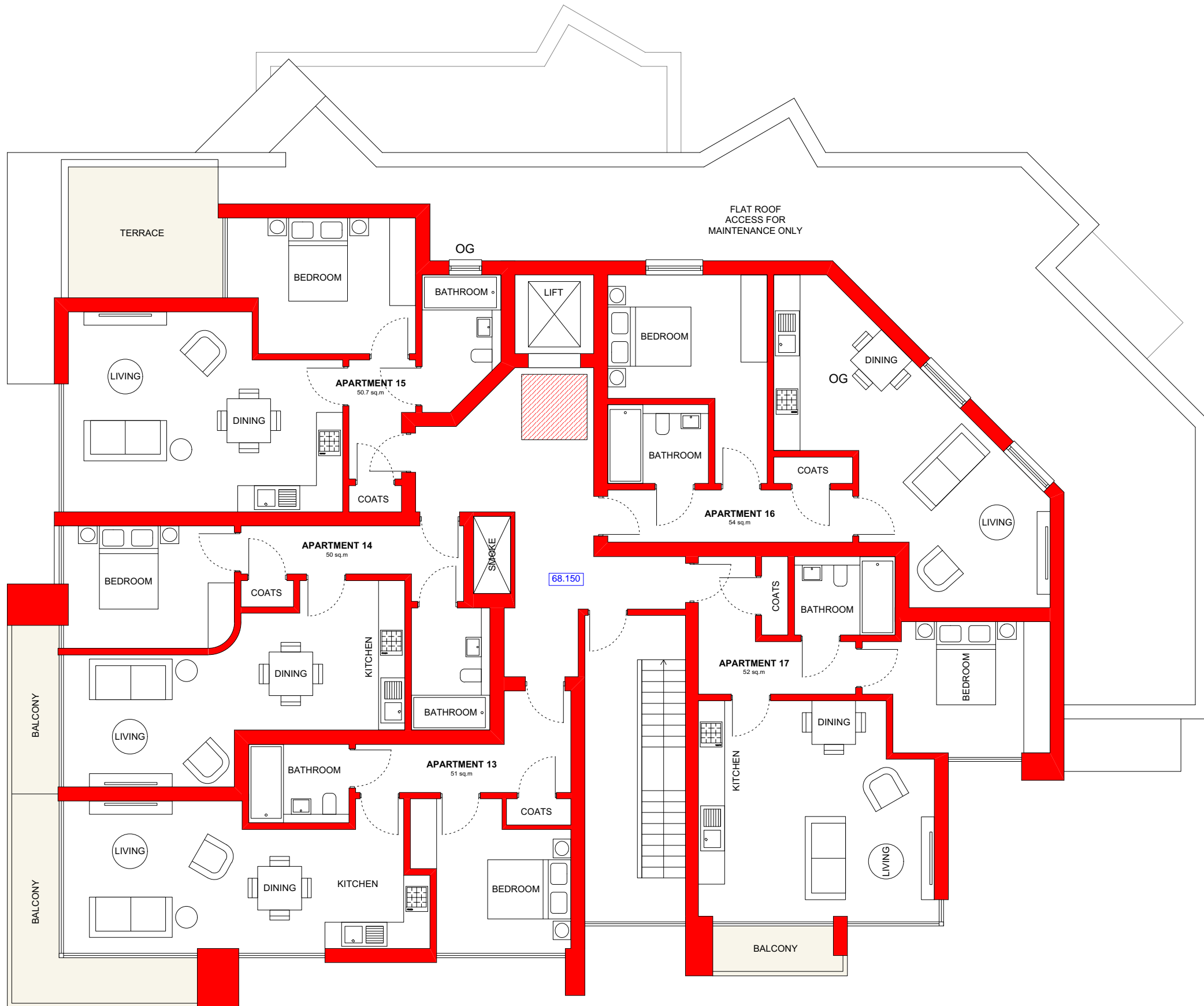
revisions: A\_20.11.19 Balconies added to studio flats 5,6 and 7 and apartment 8. SF  
 B\_23.01.27 Balcony Removed from Studio 8 & Visibility Screen added to Studio 7, Following Planning Officers comments\_RH  
 C\_11.05.20\_Floor plan updated to show omitted balconies to Studio 5. HVG



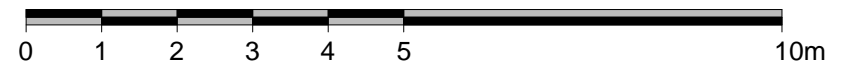


revisions: A\_22.07.19\_Apartments 11 and 12 reduced in size and converted to studio flats. SF  
 B\_20.11.19\_Balconies added to studio flats 9,10,11 and 14. Windows also added to kitchen and bathroom in studio flat 14. SF  
 C\_23.01.20\_Balcony Removed from Studio 10 & Visibility Screen added to Studio 11, Studio 14 Balcony adjusted , Following Planning Officers comments\_RH  
 D\_11.05.20\_Floor plan updated to show omitted balconies to Studio 9 . HVG

Note: Do not scale from this drawing. The contractor is to check all dimensions on site. This drawing is copyright and remains the property of David James Architects & Partners Ltd. Unauthorised use for any purpose is prohibited.

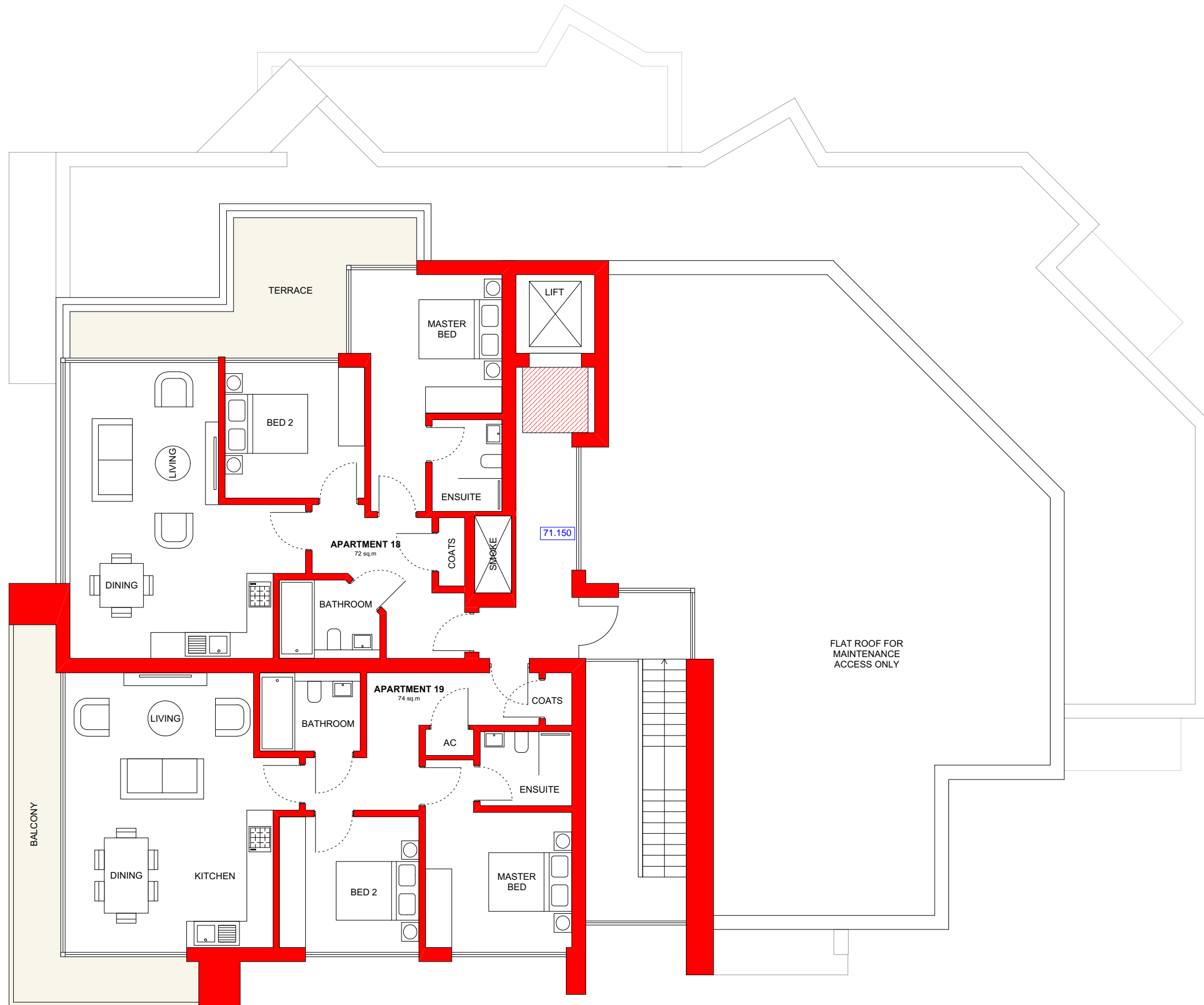


OG - Obscure Glazing



revisions: A\_22.07.19\_Apartment 16 reduced in size and layout amended. SF  
 B\_20.11.19\_Floor plan updated to show outline of additional balconies on floors below. SF  
 C\_23.01.27\_Balcony outline below updated as per Second floor, Following Planning Officers comments. RH  
 D\_11.05.20\_Roof plan updated to show omitted balconies. HVG





revisions: A\_22.07.19\_Lift and stair exterior walls aligned. SF  
B\_20.11.19\_Floor plan updated to show outline of additional balconies on floors below. SF  
C\_23.01.27\_Balcony outline below updated as per Second floor, Following Planning Officers comments\_RH  
D\_11.05.20\_Roof plan updated to show omitted balconies . HVG

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Application Address	The Goods Yard, 14 Station Approach, Broadstone, BH18 8AX
Proposal	Side and rear extension to form two additional apartments together with associated access and parking and provision of refuse collection area
Application Number	APP/19/00416/F
Applicant	Primetower Properties Ltd
Agent	Chapman Lily Planning Ltd
Date Application Valid	3 April, 2019
Decision Due Time	26 May, 2020
Extension of Time date (if applicable)	
Ward	Broadstone
Recommendation	Grant in accordance with the Recommendation below
Reason for Referral to Planning Committee	This application is brought before committee at the request of Cllr Brooke because of the impact on heritage assets and conflict with pedestrian and highway safety.
Case officer	James Gilfillan

## Description of Development

1. Planning consent is sought for a side and rear extension to form two additional apartments together with associated access and parking and provision of refuse collection area

## Key Issues

2. The main considerations involved with this application are:
  - The Principle of the development
  - Heritage issues and the character of the area
  - Access and highway safety
3. These points will be discussed as well as other material considerations below.

## Planning Policies

4. National Planning Policy Framework (February 2019)
5. Poole Local Plan (Adopted 2018)
  - PP01 Presumption in favour of sustainable development
  - PP02 Amount and broad location of development

PP07	Facilitating a step change in housing delivery
PP08	Type and mix of housing
PP27	Design
PP28	Flats and plot severance
PP30	Heritage Assets
PP32	Poole's important sites
PP33	Biodiversity and geodiversity
PP34	Transport strategy
PP35	A safe, connected and accessible transport network
PP37	Building sustainable homes and businesses

6. Broadstone Neighbourhood Plan:

BP03	Enhancing Biodiversity
BP04	Securing High Quality Sustainable Design
BP05	Balancing the Housing Stock

7. Supplementary Planning Document

SPD1	Parking & Highway Layout in Development
SPD2	Heritage Assets
SPD3	Dorset Heathlands Planning Framework (2020-2025)
SPD5	Poole Harbour Recreation SPD (2019-2024)
SPD6	Nitrogen Reduction in Poole Harbour (2017)

**Relevant Planning Applications and Appeals:**

8. 2007: Refurbishment and remodelling works to existing Public House. Demolition of south west wing and rear single storey structures, and the erection of 31 flats. **Approved.** (07/12916/007/F)
9. 2009: Sub-divide upper floors of the public house from single residential flat to three residential units, including installation of rooflights. **Approved.** (APP/09/00043/F)
10. 2020: Erect a block of 33 flats on the adjoining car park is subject to a resolution to **approve** subject to the completion of a S106 agreement, which is currently pending (APP/19/00414/P).

**Representations**

11. Letters of representations have been received in which the following concerns are expressed:
  - Lack of on site parking for staff, patrons and visitors
  - Lack of servicing and impact on highway safety
  - Exacerbating existing congestion, speeding and parking problems
  - Impact on the privacy of students at the school.

**Consultations**

12. BCP Highway Authority: Supports the scheme for delivering sufficient parking with sufficient manoeuvring and safe access.
13. The Society of Poole: Object to the scheme which exacerbates existing

issues; is out of scale with its surroundings; and fails to respect local planning requirements and the associated community strategies.

### **Constraints**

14. The site is in the Tudor and Golf Links Road Conservation Area and trees on the application site are covered by a TPO

### **Planning assessment**

#### **Site and Surroundings**

15. The site is on the east side of Station Approach at its junction with Moor Road, opposite the Junction Leisure centre, close to the centre of Broadstone. The 'Goods Yard' Public House occupies the ground floor and there are three self contained flats occupying the first floor and roofspace.
16. The site is within the Broadstone Neighbourhood Forum Plan area and is in the Tudor and Golf Links Road Conservation Area. The existing building is included on the list of buildings of local importance (locally listed). It is close to the Broadstone District Centre and central shopping area.
17. The site is surrounded by commercial and community uses comprising the leisure centre to the west; Broadstone First School to the east; residential flats and commercial premises to the north on Moor Road, and a car park serving the pub to the south.
18. There are trees along the boundary with Broadstone First School.
19. Moor Road and Station Approach are both one-way streets adjacent to the application site. The pub is serviced from its car park and there are on-street parking bays and parking restrictions on the adjoining roads.

#### **Key issues**

20. The scheme proposes a part first-floor, part 2-storey extension to the existing building to provide 2 flats, with parking; bike storage; and bin stores for the pub and flats. Access would be via the adjoining land, from Station Approach.
21. Policy PP02 sets the locations for new residential development in the plan area, identifying sequentially the Town and local centres as the most sustainable locations, followed by the sustainable transport corridors identified on the proposals map. The site falls within such a corridor and Broadstone District Centre extends to the southern boundary of the adjoining car park site. The principle of additional residential accommodation is therefore appropriate.
22. There are already three flats above the 'Goods Yard' pub and an extant scheme for 31 flats on a site comprising the adjoining car park and part of the application site. On 21st May 2020, Planning Committee resolved to grant planning permission for 33 flats on the adjoining car park subject to the completion of a S.106 Agreement. There are flats on Moor Road opposite the application site. Flats would therefore reflect and preserve the existing mixed character of the area.

23. Due to its proximity to Broadstone District Centre, new dwellings would create additional spend; vitality; and vibrancy in the centre and patronage of other community services and facilities.
24. The site falls within the 'Inner Zone' identified by policy BP5 (Balancing the Housing Stock) of the Broadstone Neighbourhood Plan. The scheme proposes a 1-bed and a 2-bed flat, wholly in accordance with the policy's requirement for smaller 1 and 2-bed dwellings to be provided within the inner zone.
25. The proposals would have minor economic benefits during construction and would generate additional patronage for nearby shops, restaurants and community facilities. It would have environmental benefits associated with the application site's proximity to a range of such facilities, significantly reducing reliance on the private car for many activities. It would have social benefits of providing additional small housing in an area of high property values where opportunities for first time buyers are low and contribute to balancing the housing stock, as proposed by the Broadstone Neighbourhood Plan.
26. The scheme however, falls within the Tudor/Golf Links Road Conservation area and proposes to extend a building included on the list of buildings of local significance. Having special regard to the significance of heritage assets, great weight should be placed on conserving such assets.
27. The building is included on the list of buildings of local importance in the Heritage Assets SPD due to its Historic and Architectural interest. It is identified as a being a positive building in the Conservation Area character appraisal and is the last remaining railway building to survive substantially intact. The nature of the works proposed would have almost no impact on the wider character and appearance of the Conservation Area, its attributes and significance, much of which is based on the setting of the Church and the spacious layout and design of houses along Lower Golf Links Road. The important consideration is preserving the contribution of the existing building to the Conservation Area by protecting and respecting its distinctive identity, design and appearance.
28. The historical significance of the building and its contribution to the social evolution and railway led development of Broadstone would not change as a result of the proposed development. The part of the site to be developed was previously occupied by single-storey outbuildings providing a skittle alley and hotel rooms, which was not original and did not positively contribute to the character or appearance of the building or the Conservation area. This part of the site does not contribute towards better defining the significance of the heritage asset nor does it contribute to the spatial or visual character of the area. The extant scheme would in any case have extended across much of this part of the site, extending close to Moor Road, albeit as a separate structure.
29. The proposed scheme would extend across the existing flat roof of the single storey element of the 'Goods Yard' fronting Moor Road to link with a 2-storey 'wing' extending along the application site's eastern boundary. An open pedestrian access would be retained at ground floor for servicing; bin

collection; and to afford residents' access to the site and building. The scale and position of the first-floor extension would be subservient to the prominent existing east facing gable but would incorporate a similar roof pitch to reflect the distinctive and dominant roof of the existing building.

30. A gable projection would sit above the pedestrian access to give a degree of legibility and significance to the feature, but its height would ensure that it would not compete with the gabled roof form of the existing building. Dormers in the roof slope would respect the scale of the roof. Although the scheme does not provide additional ground floor interaction with the street, first floor windows would provide a degree of passive surveillance and the pedestrian entrance and built form would enhance the streetscene compared with the existing fence on the pavement edge.
31. The two-storey wing extending along the east boundary would be visible in views from the east, due to the open nature of the adjacent school playground. This elevation would nevertheless be subservient in scale and form to the existing building.
32. Materials and detailing on the frontage would respect that of the existing building, ensuring the integration of the extensions. Provision is made on the street frontage for enhancing the setting of the building.
33. The layout of the proposed extension would create a central courtyard providing car parking with accessed from the adjoining car park, or from the development currently pending the grant of Planning Permission (APP/19/00414/P). This space is currently covered in self seeded vegetation following the demolition of the former skittle alley and hotel bedrooms. This hard surfaced area and car parking would not be readily visible from the public realm, and would not therefore be detrimental to the streetscene or the character of the wider area.
34. The proposals would not harm the significance of the 'Goods Yard' as a heritage asset. By respecting and preserving the historical and architectural significance of the existing building, the proposals would also preserve its contribution to the character and appearance of the Conservation Area.
35. Due to the siting of the proposed works and arrangement with surrounding buildings and their uses, there would be no impact from overshadowing; loss of outlook; loss of privacy or overbearing, on the amenities of existing or adjacent occupiers. There would be an outlook across the school playground from unit 2, though this would not in itself be detrimental to the wellbeing or safety of pupils and staff using the playground. Should the scheme on the adjoining land proceed (APP/19/00414/P), sufficient separation distance would exist to preserve the amenity of the respective occupiers.
36. The proposed flats would be within easy walking distance of the district centre with its wide variety of leisure and recreation facilities, exceptionally including a swimming pool, and the Castlemain cycleway. They have sufficient outlook, privacy and light to achieve acceptable residential amenity.

37. The scheme provides 1 parking space for each new flat and 1 for each of the existing flats, a total of 5 spaces. This is sufficient to meet the needs of the development. Access would be from Station Approach, via the adjoining site. Safe access is provided for both with and without the adjacent development proceeding, preserving highway and pedestrian safety in both instances. Bollard lighting and improvements to the existing vehicle access from Station Approach to enhance the pedestrian environment can be secured by conditions (#15).
38. The proposed point of access to the site is currently subject to a private parking restriction and is used for servicing the pub. An alternative parking bay from which to service the pub safely could therefore be secured by condition. Due to the Goods Yard's location, it is not necessary to provide customer or staff parking for the commercial premises, since public parking is available to service this and many other commercial premises in the area.
39. Concerns about existing congestion and speeding on adjoining roads would not be exacerbated by the proposed development, nor would it be reasonable to require the proposed development to mitigate those existing concerns.
40. Sufficient bin and cycle storage would be provided within the ground floor of the proposed extension to serve both the existing and proposed development. This would be accessible from Moor Road and would meet the requirements of the Council's waste collection service.
41. The scheme would result in the loss of trees from a group adjacent to Moor Road, retaining a Scotts Pine and space to accommodate replacement tree planting. A revised arboricultural method statement setting out the protection of that tree during construction can be secured by condition (#4). Replacement trees and landscape planting is proposed to the rear of the site, replacing the self seeded vegetation, which would contribute to an attractive landscaped setting for the development; form an edge to the school grounds; and contribute to bio diversity enhancement as required by the neighbourhood plan policies.
42. The proximity of the site to the District Centre, its services, facilities, leisure and social opportunities, employment and public transport would significantly reduce reliance on the private car for many journeys, limiting the carbon footprint of the occupiers. Being a new building it would be readily possible to deliver an energy efficient and sustainable development in accordance with the Building Regulations. Furthermore, a condition could be used to ensure at least 10% of the schemes energy needs are met by on site sources of renewable energy generation (#12).



### Section 106 Agreement/CIL compliance

Contributions Required			Dorset Heathland SAMM	Poole Harbour Recreation SAMM
Flats	Existing	3	@ £269	@ £96
	Proposed	2		
	Net increase	2	£538	£192
Total Contributions			£538.00 (plus admin fee)	£192.00 (plus admin fee)
CIL	Zone C		@ £115sq m	

43. Mitigation of the impact of the proposed development on recreational facilities; Dorset Heathlands and Poole Harbour Special Protection Areas; and strategic transport infrastructure is provided for by the Community Infrastructure Levy (CIL) Charging Schedule adopted by the Council in February 2019. In accordance with CIL Regulation 28 (1) this confirms that dwellings are CIL liable development and are required to pay CIL in accordance with the rates set out in the Council's Charging Schedule.

44. The site is within 5km (but not within 400m) of Heathland SSSI and the proposed net increase in dwellings would not be acceptable without appropriate mitigation of their impact upon the Heathland. As part of the Dorset Heathland Planning Framework a contribution is required from all qualifying residential development to fund Strategic Access Management and Monitoring (SAMM) in respect of the internationally important Dorset Heathlands. This proposal requires such a contribution, without which it would not satisfy the appropriate assessment required by the Habitat Regulations

45. In addition, the proposed net increase in dwellings would not be acceptable without appropriate mitigation of their recreational impact upon the Poole Harbour SPA and Ramsar site. A contribution is required from all qualifying residential development in Poole to fund Strategic Access Management and Monitoring (SAMM) in respect of the internationally important Poole Harbour. This proposal requires such a contribution, without which it would not satisfy the appropriate assessment required by the Habitat Regulations.
46. The applicant has submitted a Section 111 form and paid the relevant contributions towards Dorset Heathlands and Poole Harbour Recreation SAMM.

### **Summary**

47. The scheme preserves the mixed character of the area; the character, appearance and significance of heritage assets would be preserved; highway and pedestrian safety would be maintained; and the privacy and amenity of occupiers and neighbours would be preserved

### **Planning balance**

48. The scheme would preserve the character and appearance of the heritage assets, provides residential units in a sustainable location that has economic, environmental and social benefits.

### **RECOMMENDATION**

- 49. GRANT permission with the following conditions, which are subject to alteration/addition by the Head of Planning and Building Control provided any alteration/addition does not go to the core of the decision and the completed S111 agreement:**

1. GN150 (Time Expiry 3 Years (Standard))
2. PL01 (Plans Listing)
3. GN030 (Sample of Materials)
4. TR010 (Arb Method Statement-Submission Required)
5. LS020 (Landscaping Scheme to be Submitted) (including 6 replacement trees)
6. HW100 (Parking/Turning Provision)
7. HW230 (Permeable surfacing condition)

8. AA01 (Non standard Condition)

Prior to first occupation the bin stores shown on the approved plans shall be delivered and available for use, and shall thereafter be retained for such uses.

Reason

In the interests of highway safety and residential amenity and in accordance with the Policies PP27, PP30 and PP35 of the Poole Local Plan 2018

9. AA01 (Non standard Condition)

The measures to deliver bio-diversity enhancement shown on the approved plans shall be installed prior to first occupation of the development hereby approved and thereafter retained.

Reason

In the interests of delivering bio-diversity enhancements in accordance with PP33 of the Poole Local Plan 2018

10. AA01 (Non standard Condition)

Prior to first occupation of the flats hereby approved, the works to install the new window to flat GY1, as shown on the approved plans, shall be completed.

Reason.

In the interests of the amenity of the occupier of that property and in accordance with Policy PP27 of the Poole Local Plan 2018

11. AA01 (Non standard Condition)

Prior to the commencement of use of the parking courtyard, as shown on the approved plans, a scheme to introduce the loading bay along Moor Road, as shown on the approved plans, shall have been submitted to and approved in writing by the Local Planning Authority. The scheme shall include design details, signage and markings and costs related to the making of any Traffic Regulation Orders. The approved details shall then be implemented before use of the approved parking courtyard and thereafter retained for such servicing uses.

Reason:

In the interests of preserving safe servicing for the commercial use of site and in accordance with PP34 and PP35 of the Poole Local Plan 2018.

12. GN162 (Renewable Energy - Residential) (10%)

13. HW200 (Provision of Visibility Splays)

14. AA01 (Non standard Condition)

Prior to first occupation of any of the residential units hereby approved a scheme to provide a continuous footway across the front of the access with an appropriate vehicle crossover, as shown on the approved plan, shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of the construction specification and surface treatment and shall comply with the standards adopted by the Local Highway Authority. The agreed scheme shall then be implemented prior to first residential occupation of any of the residential units hereby approved and fully at the owners own expense, to include the provision of relevant Traffic Regulation Orders, signage and markings.

Reason:

In the interests of improved pedestrian environment and to encourage pedestrians and in accordance with Policies PP34, PP35 of the Poole Local Plan 2018

15. AA01 (Non standard Condition)

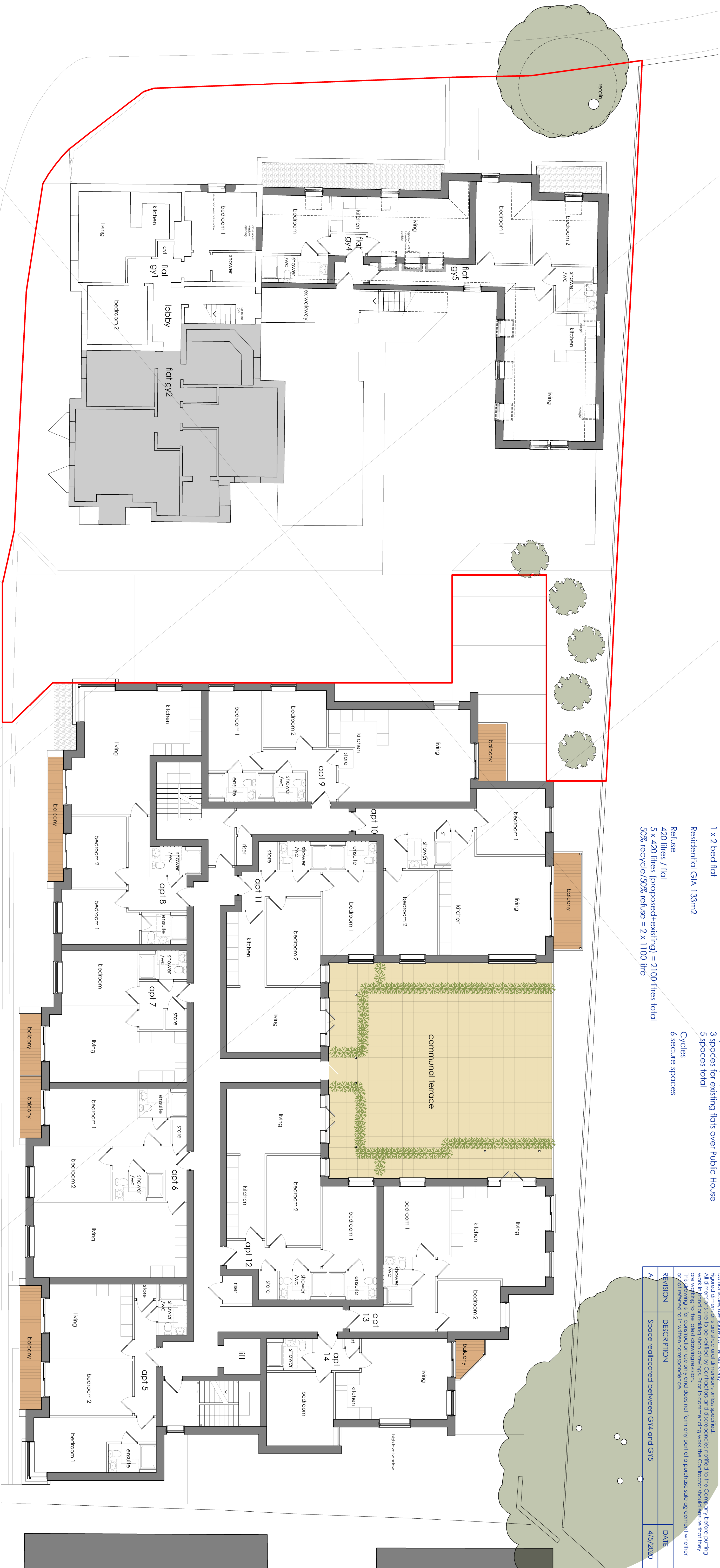
Prior to first occupation of any new residential unit hereby approved, details of a proposed scheme of lighting along the access drive and car park area, to include details and specification of the type of lighting proposed, shall be submitted to and approved in writing by the Local Planning Authority. The approved scheme shall then be implemented prior to first occupation of any new residential unit hereby approved and thereafter retained and maintained to ensure the lighting remains in working order.

Reason:

In the interests of pedestrian safety and in accordance with Policy PP34 & PP35 of the Poole Local Plan 2018

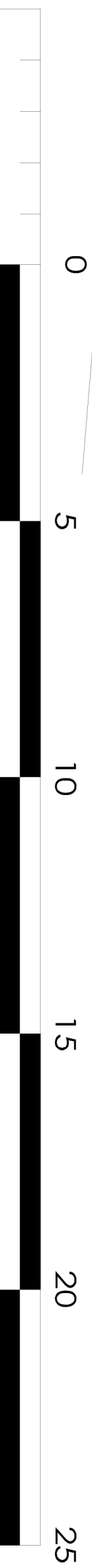
**Informative Notes**

1. IN72 (Working with applicants: Approval)
2. IN74 (Community Infrastructure Levy - Approval)
3. IN81 (SMM Approval)
4. IN84 (AA passed)



- Accommodation**
  - 1 x 1 bed flat
  - 1 x 2 bed flat
  - Residential GIA 133m<sup>2</sup>
- Parking**
  - 3 spaces / proposed flat
  - 3 spaces for existing flats over Public House
  - 5 spaces total
- Refuse**
  - 420 litres / flat
  - 5 x 420 litres (proposed+existing) = 2100 litres total
  - 50% recycle/50% refuse = 2 x 1100 litre
- Cycles**
  - 6 secure spaces

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A	Space indicated between GY4 and GY5						



NORTH

Metres



Primetower Properties Ltd  
1 Park Place, North Road, Poole, Dorset, BH14 0LY (e:01202 715571 f:01202 718971)

PROJECT

The Goods Yard  
14 Station Approach, Poole

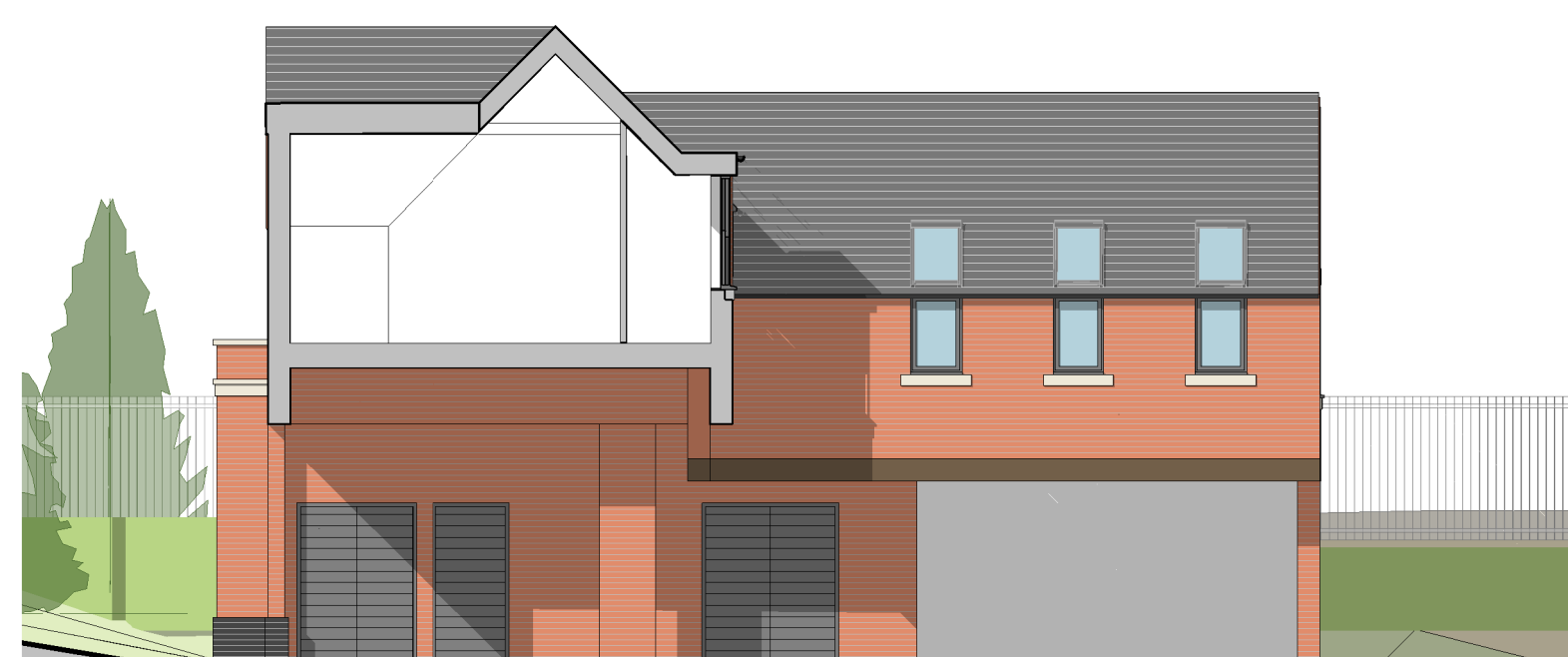
Broadstone Leisure  
Centre

Public House Extension  
First Floor Plan

DRAWING NUMBER	REVISION	SCALE	SIZE	STATUS
PT207 - 103	A	1:100	A1	Planning Application

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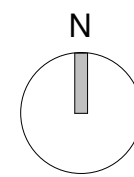
The Goods Yard Area Schedule (GIA)	
Name	Area
Pub Refuse	11 m <sup>2</sup>
Cycle Store	8 m <sup>2</sup>
Residential Refuse	8 m <sup>2</sup>
Flat B	72 m <sup>2</sup>
Flat A	40 m <sup>2</sup>



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0m 10m 20m 30m 40m 50m  
1:1250



					client Primetower Properties Ltd	drawn JL	scale As indicated	size A1
					address Goods Yard Car Park	checked IF	title Location & Block Plan	
C	Red line boundary updated		29.05.20		Station Approach, Broadstone, Poole	date 23/07/19		
B	Revised to address planning officers comments		11 12 19		BH18 8AX			
A	Revised to address planning officers comments		02 08 19		proj no. 18-101	status PL	dwg no. 101	revision C
<b>Revision</b>	<b>Notes</b>	<b>Issued by</b>	<b>Date</b>					

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Any dimensions not specified are to be interpreted as dimensions referred to in the Company below pulling work in and/or moving shop drawings. Prior to commencing work the Contractor should ensure that they are working to the latest drawing revision.

The drawing is for information only and does not form any part of a purchase sale agreement, whether or not related to or within correspondence.

Accommodation	1 x 1 bed flat	1 x 2 bed flat	Residential GIA 133m2	Parking	1 space / proposed flat	3 spaces for existing flats over Public House	5 spaces total	Cycles	6 secure spaces
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 **PRIMETOWER**  
Primetower Properties Ltd  
Park Place, North Road, Poole, Dorset. BH14 0LY Tel: 01202 715571

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# Planning Committee

Application Address	81-83 Hankinson Road, Bournemouth, BH9 1HP.
Proposal	Erection of bungalow and formation of parking spaces
Application Number	7-2019-20654-B
Applicant	Mr D Ross
Agent	Harriplan
Date Application Valid	10 December 2019
Decision Due Date	3 February 2020
Extension of Time date (if applicable)	27 <sup>th</sup> June 2020
Ward	Winton East
Report Status	Public
Meeting Date	11 <sup>th</sup> June 2020
<b>Recommendation</b>	<b>Grant, in accordance with the recommendation in the report.</b>
Reason for Referral to Planning Committee	<p>This application is brought before committee at the request of Councillor Chris Rigby for the following reasons:</p> <ul style="list-style-type: none"> <li>• Congested development</li> <li>• Loss of privacy</li> <li>• Environmental impact/impact on trees and additional hard surfacing</li> </ul>
Case Officer	Colin Tebb

## Description of Development

1. Planning consent is sought for 'Erection of bungalow and formation of parking spaces.'
2. The applicant has provided the following information:

	Existing	Proposed
Use	Rear garden to No83	New dwelling
Height	N/A	Single storey bungalow
Parking	None	2 spaces

## **Key Issues**

3. The main considerations involved with this application are:
  - Impact on character and appearance of the area;
  - Impact on neighbouring residents;
  - Impact upon living conditions;
  - Impact upon Trees;
  - Parking/Traffic/Highway Safety;
4. These points will be discussed as well as other material considerations in Paragraphs 12 to 28 below.

## **Planning Policies:**

5. Applications for planning permission must be considered having regard to the provisions of the Development Plan and any other relevant material considerations. The key document being the Bournemouth Local Plan.
6. Various Development Plan Documents (DPD) make up the Local Plan, the Bournemouth Local Plan: Core Strategy (October 2012) being the overarching document. The CS has superseded the Bournemouth District Wide Local Plan (DWLP) as the strategic policy framework for the Borough although various policies in the DWLP have been retained as 'saved' policies. The Core Strategy covers the period 2006 to 2026.

The Affordable Housing DPD (December 2009) also makes up part of the Local Plan. The National Planning Policy Framework (NPPF), as amended July 2018, sets out the Government's planning policies for England and is a material consideration in planning decisions.

The following Policies are considered relevant to the current application:

### **Core Strategy**

Policy CS1 – Presumption in Favour of Sustainable Development  
Policy CS4 – Surface Water Flooding  
Policy CS33 – Heathlands  
Policy CS39 – Designated Heritage Assets  
Policy CS41 – Quality Design

### **District Wide Local Plan**

Policy 4.25 – Landscaping  
Policy 6.8 – Infill Residential Development

### **National Planning Policy Framework**

The National Planning Policy Framework (NPPF), as amended, sets out the government strategy to achieve sustainable development. The framework is relevant to the current application and issues relating to the economy, ensuring the vitality of town centres, sustainable transport, high quality communications, housing, flooding/climate change, good design, promoting healthy communities, protecting green belt land, and conserving the natural and historic environment will be dealt with in the report where relevant.

## Other

Dorset Heathlands Planning Framework – SPD

Residential Extensions: A Design Guide For Householders - PGN

Sustainable Urban Drainage Systems (SUDS) - PGN

## Relevant Planning Applications and Appeals:

- 7-2017-9396-A: New bungalow land rear of 85 Hankinson Road. Approved, and currently under construction.
- 7-2011-20654-B: New bungalow, land rear of No81 Hankinson Road. Approved – subsequently built and address is No81A.

## Representations:

7. Site notices were placed in the vicinity of the site on 17<sup>th</sup> December 2019, with an expiry date for consultation of 10<sup>th</sup> January 2020.
8. The application has been called in, and 15 representations have been received objecting to various aspects, including:
  - *Loss of the green open space and trees, and wildlife in this wildlife corridor;*
  - *A further dwelling here, along with other developments to the rear of Hankinson Road and Maxwell Road, will cause further overlooking of these neighbouring properties;*
  - *The additional driveways and car movements between properties will cause additional noise and disturbance to neighbours;*
  - *There will also be increased pressure for parking on Hankinson Road;*
  - *Impact upon two major trees on an adjacent site;*
9. The Member call-in is particularly concerned regarding the proposed bungalow having a congested impact on its surroundings, direct views into the rear of No51, 53 and 55 Maxwell Road, plus potential loss of trees, to create the garden, plus the impact of further hard landscaping upon what is free draining land These concerns are referred to in greater detail below under the impact upon neighbours and the other headings where necessary.

## Consultations:

10. Local Highway Authority - no objection:

*“The site is located in parking Zone 3; from the appropriate tables in the SPD both the proposed and donor dwellings have a shortfall of 0.5 spaces, however in this instance, given the existing highway conditions and the existing house has 2 spaces, the shortfall can be safely accommodated on street.*

*A turning head provision is proposed on site allowing vehicles for both the new and donor dwellings to enter and egress in forward gear, however tandem car parking layout is not ideal but given that the occupiers are in the same building it is not an issue.”*

11. Waste & Recycling - no objection, provided a condition is imposed requiring the presentation point shown on the submitted plans to be installed and utilized.

12. Trees - An ARB report and AMS were provided, and the Landscape and Tree Officer has no objection provided the requirements within this report are implemented, as discussed (and conditioned) below.

**Constraints:**

13. No site-specific constraints

**Planning Assessment:**

**Site and Surroundings**

14. The site is located within the Winton area of Bournemouth. The area is residential in character and contains principally detached two storey dwellinghouses with generally long rear gardens but sometimes limited spacing between. There have been some infill developments of bungalows, notably to the rear of numbers 81, 85 and 97-99 for example.

**Key Issues:**

**Impact upon the character and appearance of the area:**

15. There have been other similar 'backland' dwellings approved to the rear of Hankinson and Maxwell Road, including either side of the application site, with new bungalows approved in 2011 (No.81) and more recently in 2017 (No.85), with the latter currently under construction. So, the material considerations are almost identical to those applications, as this dwelling would also be a single storey bungalow with associated parking and access between dwellings fronting Hankinson Road. The area is residential in character and contains principally detached two storey dwellinghouses with generally long rear gardens, and in the case of Hankinson Road the level of the gardens dropping down from north to south by almost 6 metres.
16. This further proposal seeks to erect a similar two bedroom bungalow to the rear of No.83 Hankinson Road, utilising the access road currently used to reach the bungalow to the rear of No.81, (No.81a) so becoming a shared access and turning head serving both existing and potential properties. This proposed bungalow would have a similar almost L – shape footprint, with its rear wall in line with the existing rear wall of No.81a. The bungalow would measure 2.5 metres to the eaves and 5 metres to the ridge with a hipped roof.
17. The houses in Hankinson Road around and including the proposal site have relatively long rear gardens, which slope down towards the rear gardens of houses facing Maxwell Road. Given that the proposed bungalow would be sited at a considerably lower level than the houses in Hankinson Road, it is considered that it would not be visually intrusive from either nearby gardens or from the surrounding streets. The separation distance of approximately 15.5 metres between No.83 Hankinson Road and the proposed bungalow is sufficient to ensure that it would not create a cramped relationship, and is very similar to others approved in the area.
18. In design terms, the proposal would therefore not have a noticeable impact on the visual appearance of the locality or character of the area. The traditional design approach would integrate with others in the area. The proposal retains good



spacing to all neighbouring buildings and would therefore not constitute an overdevelopment of the site.

19. A potential issue in this regard is that there are Laurel bushes/shrubs on the site which will be lost, although they are not overly large specimens. Other larger trees are outside of the application site, however a detailed Arboricultural Impact Assessment and report has been submitted alongside this application, which in turn has been considered by the Tree officer, and found to be acceptable, (as discussed below) in terms of protection of existing trees outside the site. As such, this proposal would not detract from the character and appearance of the area, or conflict with Policy CS41 as such.

#### **Impact on neighbouring residents:**

20. The application property is flanked on both sides by neighbouring dwellings now, with a dwelling on both sides at No.81a and now No.85a. A concern has been raised that there will be overlooking of properties in Maxwell Road, however the proposed bungalow itself is single storey and consequently the overall height and design means that it would not be visually intrusive from the rear of neighbouring properties. There is a difference in levels, but the separation distance is considered acceptable, and there is also a degree of boundary screening to the rear boundaries.
21. Equally, the typical 1.8m high close boarded fence would also prevent any issues in terms of the inter-relationship between the new bungalow in both Maxwell Road and Hankinson Road, as was the case with the new bungalows, in their determination and subsequent approval. In this case, it is also recommended that the usual Permitted Development right for roof alterations be removed by condition, and fencing details too, so these are recommended under Conditions 5 and 7 respectively.
22. With regards to the specific issue of the distance between the new bungalow and Maxwell Road properties, with a rear garden 7 metres deep and the rear gardens of Maxwell Road properties being circa 28 metres, then there would be an overall back to back distance of 35 metres. This would far exceed the recommended 21m back to back distance in the Residential Development SPD (between two storey dwellings), and as such the proposal would not conflict with Policy CS41 in this respect either.

#### **Living conditions for future occupants:**

23. The proposed bungalow has two bedrooms and an acceptable level of internal space and adequately sized rooms too, along with adequate amenity space, with the external space relating well to the main internal room living room, as recommended in the Residential Development SPD too.

#### **Impact on Trees:**

24. There is a large clump of self-seeded Laurel bushes which will need to be removed, however these are not larger trees which are having an impact upon the verdant nature of the area and their removal will not therefore fundamentally change the broader pleasant nature of the area. There is a mature Lime tree in an adjacent garden serving Maxwell Road, which has been picked up in the detailed

Arboricultural Impact Assessment which has been undertaken. Consequently, as a result, the measures recommended in the AIA and AMS are covered by relevant and enforceable conditions (Condition 8) then the Tree officer is indeed satisfied this bungalow will not transgress into its Root Protection Area or adversely affect this mature Lime tree.

#### **Parking/Traffic/Highway Safety:**

25. The application site consists of a deep rear garden serving a frontage dwellinghouse, which appears to be converted into two smaller ground and first floor flats. The existing access serving No.81a will become shared with this further bungalow, with a shared turning head too. Two car parking spaces are proposed for the new bungalow. The Local Highway Authority have confirmed they are satisfied the proposal will not result in additional or unacceptable increase in levels of on-street parking. Bin storage and a presentation point is shown on the plan, although details would be required as part of a condition (No.9).

#### **Drainage:**

26. Policy CS4 of the Core Strategy requires details of surface water drainage. The site is clearly sloping significantly from north to south, with a number of new hard surfaced areas, however the area is not located in a location liable to Pluvial flooding, and a standard surface water condition (No.10) is recommended. The plans show a soakaway location to the rear garden.

#### **Heathland Mitigation:**

27. As the site is within 5km of a designated Dorset Heathlands SPA (Special Protection Area) and Ramsar Site, and part of the Dorset Heaths candidate SAC (Special Area of Conservation) which covers the whole of Bournemouth. As such, the determination of any application for an additional dwelling(s) resulting in increased population and domestic animals should be undertaken with regard to the requirements of the Habitat Regulations 1994. It is considered that an appropriate assessment could not clearly demonstrate that there would not be an adverse effect on the integrity of the sites, particularly its effect upon bird and reptile habitats within the SSSI.
25. Therefore, as of 17<sup>th</sup> January 2007 all applications received for additional residential accommodation within the borough is subject to a financial contribution towards mitigation measures towards the designated sites. A capital contribution is therefore required and a signed legal agreement has been drafted to provide this contribution, and this is near sealing, so this application will be subject to the completion, signing and sealing of a S106. The contribution in this instance is £365 plus a £75 admin fee.

#### **Community Infrastructure Levy:**

26. The development proposal is liable to a community infrastructure levy charge, and the Applicants will be informed of this figure assuming Planning Permission is granted.

## Summary

27. The proposal would:

- follow very similar approvals for infill development either side of this potential plot, with an identical single storey designed 'bungalow';
- not overlook neighbours, or result in an over dominant development due to the single storey nature of the proposal;
- provide an acceptable standard of living conditions for future residents;
- not adversely affect the mature Lime tree in an adjacent garden;
- utilise an existing access, to become a shared one which raises no highway objection, with sufficient parking for the development;
- would make a contribution towards local housing delivery.

## Planning Balance

28. Therefore, having considered the appropriate development plan policy and other material considerations, including the NPPF, it is considered that subject to (A) signing and sealing of a S106 and (B) compliance with the conditions attached to this permission, the development would be in accordance with the Development Plan, would not materially harm the character or appearance of the area or the amenities of neighbouring and proposed occupiers and would be acceptable in terms of traffic safety and convenience. The Development Plan Policies considered in reaching this decision are set out above.

## Recommendation

29. **GRANT permission with the following conditions, which are subject to alteration/addition by the Head of Planning Services provided any alteration/addition does not go to the core of the decision and the completion, signing and sealing of a Section 106 agreement.**

### **Section 106 terms:**

Financial contribution towards Heathlands Mitigation Measures - £365 + £75 admin fee

### **Conditions**

#### **1. Development to be carried out in accordance with plans as listed**

The development hereby permitted shall be carried out in accordance with the following approved plans: 4381/01, 02 and 03.

**Reason:** For the avoidance of doubt and in the interests of proper planning.

#### **2. Access/Turning/ Parking**

Prior to the occupation of the development hereby approved, the access and areas for turning, parking including the marking out of spaces shown on the approved plans shall be constructed in accordance with the approved details and permanently retained and kept available for the residents and visitors of the development hereby permitted, at all times.

**Reason:** In the interests of highway safety and in accordance with Policies CS14 and CS16 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **3. Prior Approval of Materials (Bricks & Tiles)**

Details/samples of the bricks, tiles, including hanging tiles, windows, doors, and other materials to be used on the external surfaces of the proposed development shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of any superstructure works on site. Development shall be carried out in accordance with the approved details.

**Reason:** To ensure a satisfactory visual relationship between the existing and the new development in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **4. On site working hours (inc demolition) restricted when implementing permission.**

All on-site working, including demolition and deliveries to and from the site, associated with the implementation of this planning permission shall only be carried out between the hours of 8 a.m. and 6 p.m. Monday - Friday, 8 a.m. and 1 p.m. Saturday and not at all on Sunday, Public and Bank Holidays unless otherwise agreed in writing with the Local Planning Authority.

**Reason:** To safeguard the amenities of occupiers of adjoining and nearby properties and in accordance with Policies CS14 and CS38 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **5. No permitted Development for windows, Dormer Windows**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development Order) 2015 (or any order revoking and re-enacting that Order with or without modification), no additional windows shall be installed or dormer windows shall be constructed without the grant of further specific planning permission from the Local Planning Authority.

**Reason:** To avoid loss of privacy for adjoining properties in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **6. No Permitted Development for Enlargements of the dwelling(s)**

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development Order) 1995 (or any order revoking and re-enacting that Order with or without modification), no enlargements of the dwelling shall be constructed without the grant of further specific planning permission from the Local Planning Authority.

**Reason:** To enable the Local Planning Authority to retain control over the development of the site in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

### **7. Boundary Treatment (Location shown on plan & type on forms)**

Prior to the occupation of the development hereby approved fences and walls shall be erected in the positions shown on the approved plans of the type and dimensions specified. The fence(s)/wall(s) shall be thereafter retained unless otherwise agreed in writing by the Local Planning Authority.

**Reason:** In the interests of amenity and privacy and in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

## **8. Implementation of the approved Arboricultural Method Statement**

The tree protection measures as detailed in the Arboricultural Method Statement dated 20<sup>th</sup> February 2020 and prepared by KJC Consultancy shall be implemented in full and in accordance with the approved timetable and maintained and supervised until completion of the development.

**Reason:** To ensure that trees and other vegetation to be retained are not damaged during construction works and to accord with Policy 4.25 of the Bournemouth District Wide Local Plan (February 2002).

## **9. Provision of bin presentation point**

The bin presentation point hereby approved shall be constructed in accordance with the approved details prior to the occupation of the proposed development and shall be retained and maintained for that use thereafter.

**Reason:** To preserve the visual amenities of the locality in accordance with Policy CS41 of the Bournemouth Local Plan: Core Strategy (October 2012).

## **10. Surface Water Drainage (SuDS Implementation)**

Before the commencement of development, unless otherwise agreed in writing by the Local Planning Authority, a scheme for the whole site providing for the disposal of surface water run-off and incorporating sustainable urban drainage systems (SUDS), shall be submitted to and approved in writing by the Local Planning Authority. The drainage works shall be completed in accordance with the approved details prior to occupation of the development or in accordance with a timetable to be agreed in writing by the Local Planning Authority. The scheme shall include the following as appropriate:

- a) A scaled plan indicating the extent, position and type of all proposed hard surfacing (e.g. drives, parking areas, paths, patios) and roofed areas.
- b) Details of the method of disposal for all areas including means of treatment or interception for potentially polluted run off.
- c) Scaled drawings including cross section, to illustrate the construction method and materials to be used for the hard surfacing (sample materials and literature demonstrating permeability may be required).

**Reason:** To provide satisfactory drainage for the development in accordance with Policy CS4 of the Bournemouth Local Plan: Core Strategy (October 2012) and in order to achieve the objectives set out in the Local Planning Authority's Planning Guidance Note on Sustainable Urban Drainage Systems.

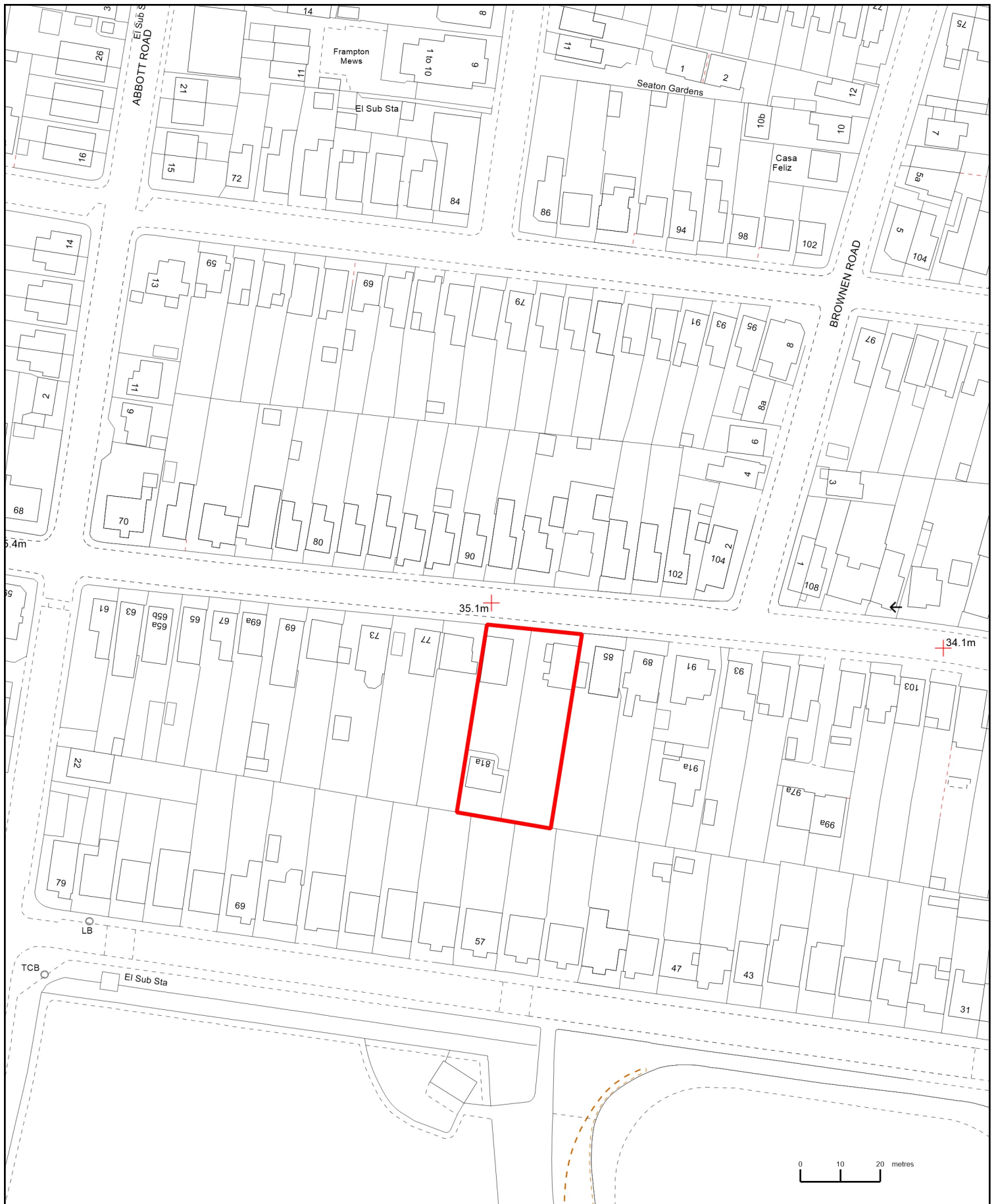
## **11. Informative Note: CIL liable development**

INFORMATIVE NOTE: This permission is subject to the Community Infrastructure Levy (CIL) introduced by the Town and Country Planning Act 2008. A CIL Liability Notice has been issued with this planning permission that requires a financial payment on commencement of development. Full details are explained in the notice.

**Background Documents:**

Case File – ref 7-2019-20654-B

NB: For full details of all papers submitted with this application please refer to the relevant Public Access pages on the council's website.



81-83 Hankinson Road



Scale: 1:1250 @ A4  
 Date: 01 June 2020

Creator: USER NAME

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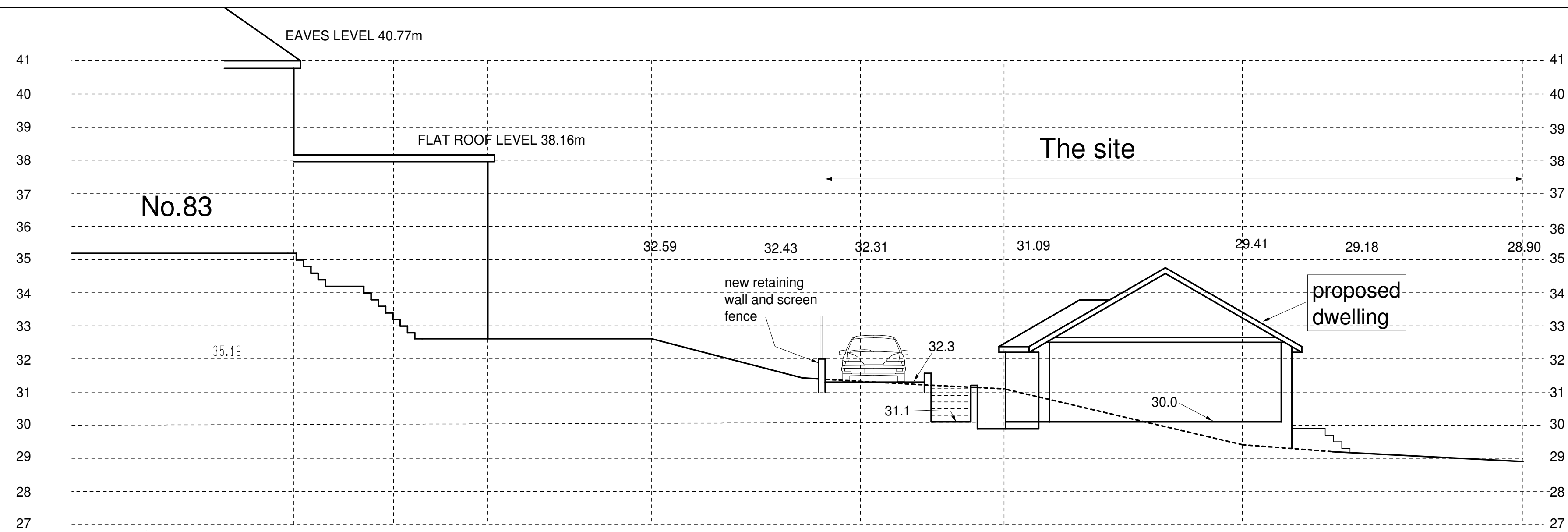
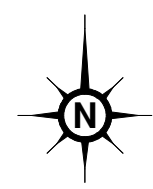




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Location Plan 1:1250

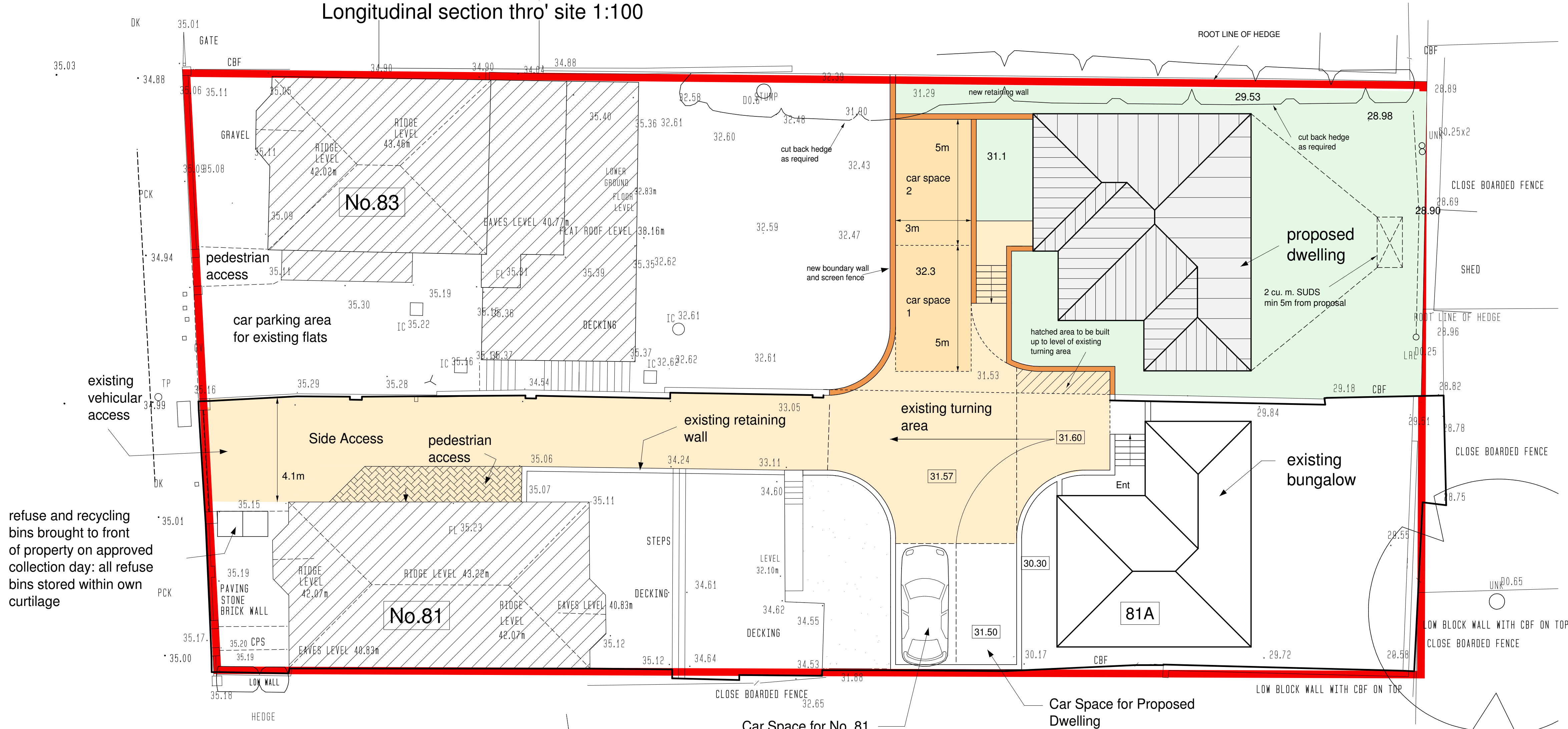


Longitudinal section thro' site 1:100

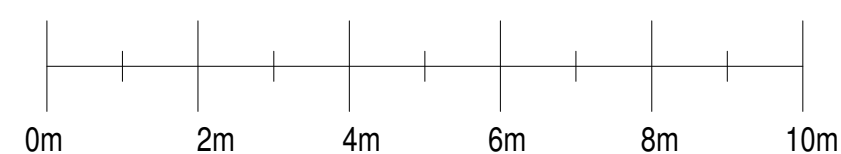
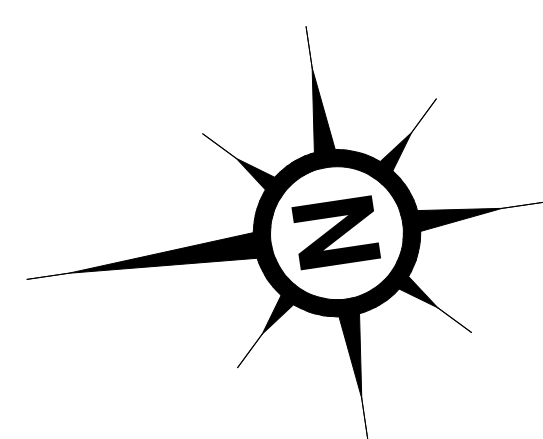
SUDS soakaway calculation:  
Proposed dwelling: 77 sq m.  
Hardstanding: All brick paviors  
Total = 77  
 $77 \times 0.05/3 = 1.3$  cu. m. NOTE 2 cu. m. provided

Suds Soakaway Management:  
Removal of silt and vegetation around components to be carried out on a monthly basis.  
Inspection of inlets/outlets, litter and grass cuttings to be carried out on a monthly basis

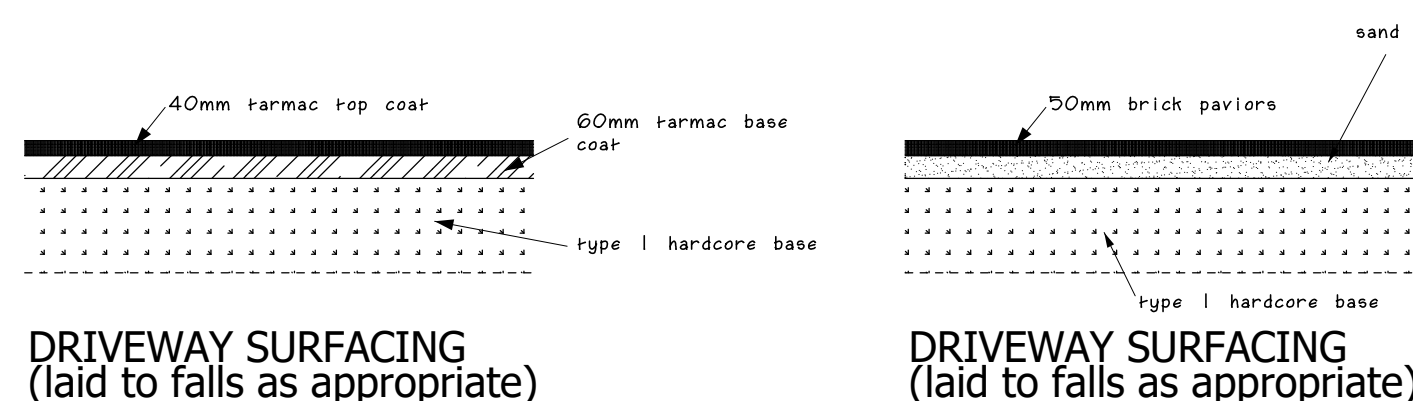
NOTE: SUDS soakaway system to be of grate design and layout: Proposed sizes to be dependant on suitable ground conditions: to be investigated on site prior to commencement



Site Plan 1:100



1:100 scale bar at A1



**PROPOSED DWELLING AT  
r/o. 83, HANKINSON ROAD,  
CHARMINSTER, BOURNEMOUTH  
FOR P. BUTTERWORTH ESQ.**

**SITE PLANS AND SECTIONS  
(detailed planning: sheet 3 of 3)**

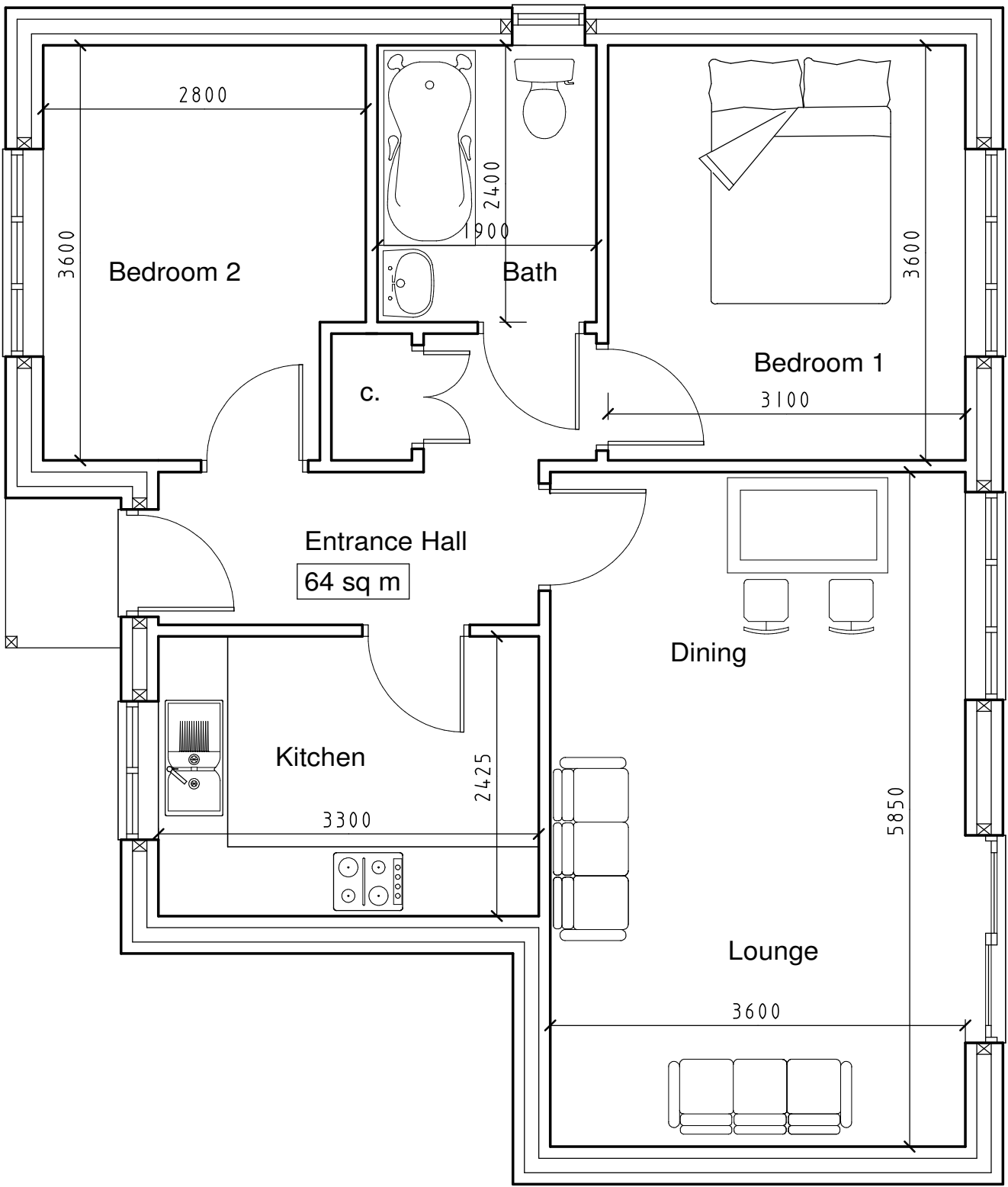
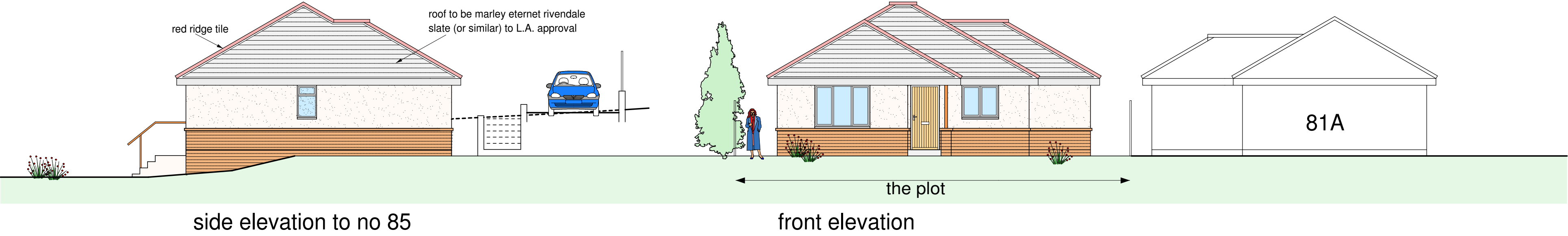
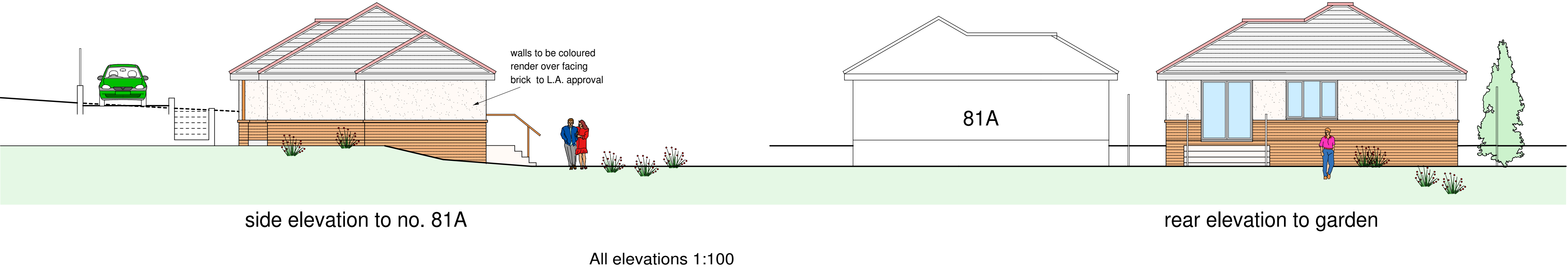
DRAWN BY:	J.G.H.
DRAWING NO:	4381:3
DATE:	NOVEMBER 2019
SCALE:	1:100

NOTE:
1. WORK TO FIGURED DIMENSIONS ONLY. DO NOT SCALE FROM DRAWINGS. SPECIFIC DIMENSIONS AVAILABLE ON REQUEST.
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5.

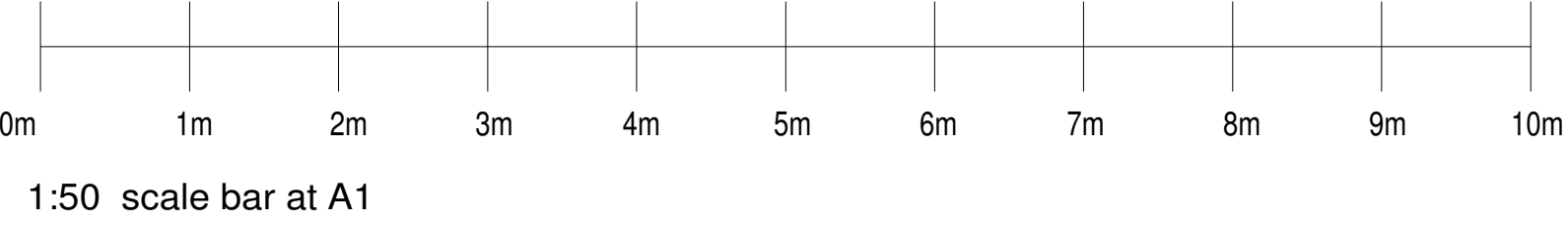
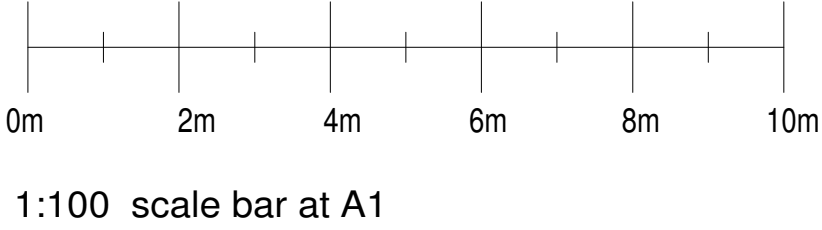
<b>h a r r i p l a n</b>	
CHARTERED BUILDING ENGINEERS	
25 ABBOTT ROAD, CHARMINSTER	
BOURNEMOUTH BH9 1EU. TEL/FAX 01202 549 590	
E-Mail: jon@harriplan.com	JON HARRISON M.C.A.B.E.



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Floor plan 1 : 50



DRAWN BY:	J.G.H.
DRAWING NO:	4381:2
DATE:	NOVEMBER 2019
SCALE:	1:50 & 1:100

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5.

<b>harriplan</b> CHARTERED BUILDING ENGINEERS 25 ABBOTT ROAD, CHARMINSTER BOURNEMOUTH BH9 1EU. TEL/FAX 01202 549 590	
E-Mail: jon@harriplan.com	JON HARRISON M.C.A.B.E.



PROPOSED DWELLING AT  
r/o. 83, HANKINSON ROAD,  
CHARMINSTER, BOURNEMOUTH  
FOR P. BUTTERWORTH ESQ.

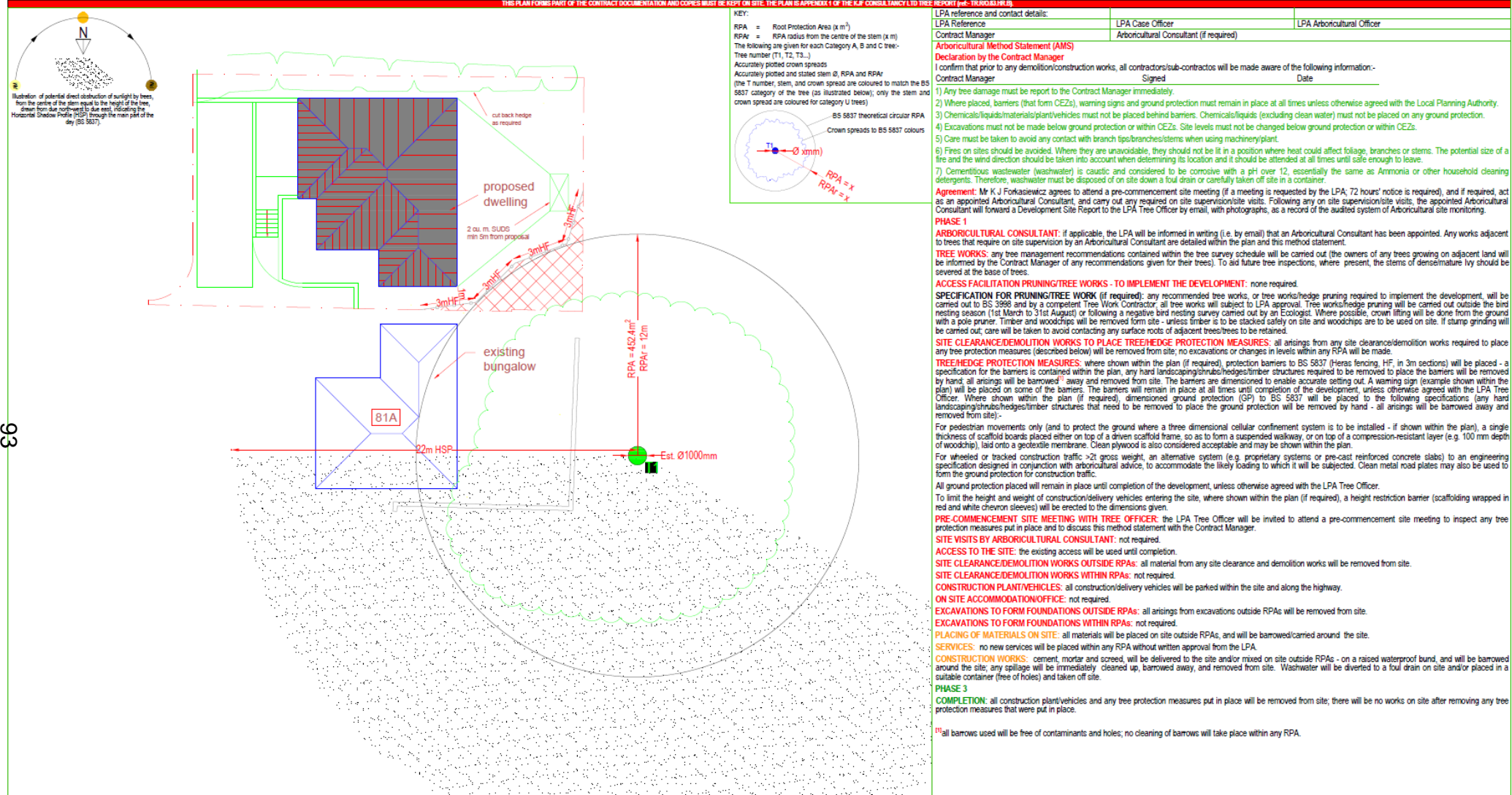
SITE PLANS AND SECTIONS  
(detailed planning: sheet 2 of 2)

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TREE SURVEY SCHEDULE TO BS 5837: 2012, N/A = NOT APPLICABLE (\*trees growing on adjacent land - surveyed from within the site only) Columns: sequential tree reference numbers (T1, T2, T3 etc), Common and botanical name (e.g. Common oak, *Quercus robur*), Life stage (Dead, Young, Semi-mature, Maturing, Mature, Over-Mature, Veteran), Est. height (e.g. 18m), Potential ultimate height (e.g. ≥ 25m), Spread (N, E, S, W), % Est. spread (e.g. at ≈ 80%), Stem Ø (e.g. 600mm), RPA (Root Protection Area e.g. 163m<sup>2</sup>), RPA<sub>r</sub> (Root Protection Area radius from the centre of the stem e.g. 7.2m), Height to 1st branch over the site & to lowest foliage over the site - at the extremity of the branch spread (e.g. 2.5m and 2.2m), Crown to stem ratio (e.g. 80%), Physiological and structural condition, form & quality - at the time of the survey, Management recommendations regardless of a development (e.g. remove dead branches ≥ 50mm Ø), Est. remaining years (e.g. ≥ 40 years), Category & Sub category (e.g. A2), Colour (e.g. Light Green). The tree survey was carried out in February 2020. NB: before carrying out any recommended tree works, checks must be made with the Local Planning Authority (LPA) regarding Tree Preservation Orders/Conservation Areas (both of which afford protection to trees).

Tree Common & botanical names	Life stage	Est. height (m)	Potential ultimate height (m)	Spread (N,E,S,W)	% Est. spread	Stem Ø (mm)	RPA (m <sup>2</sup> )	RPA <sub>r</sub> (m)	Height to 1st branch over the site & to lowest foliage over the site (m)	Crown to stem ratio (%)	Physiological/structural condition, form & quality - at the time of the survey	Management recommendations regardless of a development (e.g. remove dead branches ≥ 50mm Ø)	Est. remaining years	Category & sub-category	Colour (spread & stem)



scale bars at 1m and 5m (10m lengths):

1:50  
1:100  
1:200  
1:500

**KEY & LEGEND:**

Location of tree/hedge protection barriers

Construction Exclusion Zones - CEZ (where formed)

Buildings/structures to be demolished (if required)

Ground protection (if placed) (construction staff access only)

GP

Ground protection areas (construction traffic >2t gross weight)

No-dig and 3D Cellular Confinement System (if placed)

80mm Ø perforated tubing laid within RPAs (if placed)

BS 5837 PROTECTIVE BARRIER SPECIFICATION (NTS) (reproduced with BS's permission)

BS 5837 theoretical circular RPA

Crown spreads to BS 5837 colours

RPA = 452m<sup>2</sup>

RPA<sub>r</sub> = 12m

Est. Ø1000mm

22m HSP

existing bungalow

proposed dwelling

cut back hedge as required

2m SUDS min 5m from proposal

3mHF

3mHF

3mHF

3mHF

**AS PROTECTIVE FENCING WARNING SIGN (NTS)**

**KEEP OUT**

(AVOID ANY OVERHANGING BRANCHES)

**CONSTRUCTION EXCLUSION ZONE**

NO LIQUIDS / MATERIALS / PLANT TO BE STORED WITHIN THE FENCED OFF AREA

THE PROTECTIVE FENCING MUST REMAIN IN PLACE AT ALL TIMES

(K/J CONSULTANCY LTD 07763-121402)

**REVISIONS:**

DATE	DETAILS	REV.

**CLIENT:** Mr D Ross

**SITE:** R/O 83 Hankinson Road, Barrismouth B19 1HP

**DRAWING TITLE:** Appendix 1. Tree Protection Plan

**DATE:** 10.02.20

**SCALE:** 1:100 @ A1

**DRAWN BY:** K J Fortkiewicz

**CHECKED BY:** JF

**DRAWING No:** TFP/R/O.83.HR.B

**REV:**

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Application Address	134 High Street, Poole, BH15 1DN
Proposal	Change of Use of part ground floor, part first floor, and second and third floors from D1 Use to create 6no. flats (C3 Use)
Application Number	APP/19/01563/F
Applicant	Myddelton & Major
Agent	Simpson Hilder Associates
Date Application Valid	20 January, 2020
Decision Due Time	29 May, 2020
Extension of Time date (if applicable)	29 May, 2020
Ward	Poole Town
Recommendation	Grant in accordance with the Recommendation in the report
Reason for Referral to Planning Committee	This application is brought before committee because the Head of Planning does not consider it prudent to exercise delegated powers because of the Transportation issues raised by the proposals.
Case officer	Monika Kwiatkowska

## Description of Development

1. Planning consent is sought for a change of use of part ground floor, part first floor, and second and third floors from D1 Use to create 6no. flats (C3 Use)

## Key Issues

2. The main considerations involved with this application are:
  - The principle of development and policy context
  - Impact on the character and appearance of the Town Centre Heritage Conservation Area, including that of the locally listed building
  - Impact on the neighbouring amenity
  - Impact on highway safety and parking provision
  - Implications of the proposal within the future flood zone
  - Sustainability Issues
  - Biodiversity issues

- SAMM and CIL Compliance

3. These points will be discussed as well as other material considerations below.

## **Planning Policies**

### **4. Poole Local Plan 2018**

PP01 Presumption in Favour of Sustainable Development  
 PP02 Amount and Broad Location of Development  
 PP03 Poole Town Centre Strategy  
 PP06 High Street, Quay and Old Town  
 PP07 Facilitating a Step Change in Housing Delivery  
 PP08 Type and Mix of Housing  
 PP26 Sports, recreation and community facilities  
 PP27 Design  
 PP28 Flats and Plot Severance  
 PP30 Heritage Assets  
 PP31 Poole's Coast and Countryside  
 PP32 Poole's Nationally, European and Internationally Important Sites  
 PP33 Biodiversity and Geodiversity  
 PP34 Transport Strategy  
 PP35 A Safe, Connected and Accessible Transport Network  
 PP37 Building Sustainable Homes and Businesses  
 PP38 Managing Flood Risk  
 PP39 Delivering Poole's Infrastructure

### **5. Poole Quays Forum Neighbourhood Plan (adopted February 2017):**

PQF03 High Quality Design  
 PQF08 Creating a More Vibrant High Street During the Day and Night  
 PQF09 The layout and appearance of Poole High Street

### **6. Supplementary Planning Documents**

Parking and Highway Layout in Development SPD (Adopted July 2011)

The Dorset Heathlands Planning Framework 2020-2025 SPD (Adopted March 2020)

Nitrogen Reduction in Poole Harbour SPD

Poole Harbour Recreation 2019-2024 Supplementary Planning Document (SPD)

Town Centre Heritage Conservation Area Appraisal and Management Plan SPD (2015)

### **7. National Planning Policy Framework (February 2019)**

## **Relevant Planning Applications and Appeals:**

**2017:** Elevational alternations. **Approved.** (APP/17/01116/F).

**2017:** Subdivision of ground floor to create two A3 retail uses; New shopfront/entrance doors to retail unit 2; New entrance to upper floors off North Street; New doors to rear service yard area; and Change of use of part ground floor & upper floors to D1 (Place of Worship) with ancillary office space. **Approved. (APP/17/00212/F)**

**2017:** Shopfront alterations (front and side elevations) and 2 No. condenser units. **Approved. (APP/17/00407/F)**

**2017:** Prior Approval application for the conversion of B1 offices on upper floors to residential. **Withdrawn** prior to being determined. **(APP/17/01120/PA)**

## **Representations**

8. A representation has been received in which the following concerns are expressed:
- the increased traffic in the area caused by additional car ownership associated with the flats
  - the increased air pollution associated with the additional car journeys

## **Consultations**

9. BCP Highways Authority - objects to the proposal on the grounds of highway safety arising from the lack of car parking to serve the proposed flats
10. Environment Agency - supports the proposal
11. BCP Waste Collection Authority - supports the proposal

## **Constraints**

12. No 134 High Street is a locally listed building within the Town Centre Heritage Conservation Area.
13. The site forms part of the Town Centre Primary Retail Frontage; is located within an area at risk of future flooding; and is in the area covered by the Poole Quays Forum Neighbourhood Plan.

## **Planning assessment**

### **Site and Surroundings**

14. The application site is occupied by a prominent 2 and 4-storey locally listed building at the corner of High Street and North Street, within both the pedestrianised area and the Town Centre Heritage Conservation Area. A service area to the rear is accessed from Vanguard Road and there is also pedestrian from North Street. The site forms part of the Town Centre Primary Retail Frontage identified by the Poole Local Plan.

15. The building was previously occupied by a fast-food outlet and a cafe. Planning permission was granted in 2017 (APP/17/00212/F) for D1 use (Non Residential Institutions) on the upper floors and the Church of Scientology now occupies part of the first floor. The remaining upper floors have remained vacant. There is an A3 unit (Café 1) on the ground floor, fronting High Street.
16. A second ground-floor unit to the rear of the building is also occupied by the Church of Scientology, albeit without the benefit of planning permission for the change of use from Class A3 to Class D1 use that this represents.
17. Surrounding properties are predominantly commercial and retail although there are some residential buildings and residential use of upper floors. There are several listed and locally listed buildings in the immediate vicinity.

## **Key issues**

### The principle of development and policy context:

18. Policy PP06 of the Poole Local Plan aims to support the vibrancy of the High Street and make use of upper floors and the rear of commercial properties for new homes. This is reflected in Policy PQF8 of the Poole Quays Forum Neighbourhood Plan, which encourages redevelopment of under-utilised sites for mixed use development to include a proportion of high quality residential floor space on the upper floors to assist in re-connecting Lagland Street with the High Street.
19. Policy PP02 states that the majority of new housing should be directed to the most accessible locations within Poole. These are the town centre, district and local centres and sustainable transport corridors which connect the town centres with local centres and provide bus services, cycling and walking facilities, enabling the residents to access key facilities and services without needing to travel by car. The application site falls within the Town Centre, sequentially the most sustainable location within this hierarchy.
20. The proposed conversion of the upper floors of the building into six flats, with their service access; bin storage; and cycle store on the ground floor is therefore in accordance with the provisions of Policies PP06; PQF8; and PP02.
21. Policy PP26 of the Poole Local Plan states that the Council seeks to retain sites currently or last used as community facilities. These include, among others, places of worship (D1 use).
22. Planning permission was granted in 2017 for a change of use of all upper floors to Class D1 use and this includes use 'for or in connection with public worship or religious instruction'. The Church of Scientology occupies part of the first floor, towards the rear of the site, and would not be affected by the proposals. The remaining upper floors have remained vacant and have never been occupied by any Class D1 use. The proposals therefore comply with the provisions of Policy PP26.

23. The proposed scheme would bring currently vacant parts of the building into active use; providing 6 flats whilst retaining the existing D1 and A3 uses and continuing their contribution to the vitality of the High Street, in line with the adopted policies.

Impact on the character and appearance of the Town Centre Heritage Conservation Area and the locally listed building:

24. Policies PP28 and PP27 of the Poole Local Plan seek to ensure that all new development reflects the character of its setting and achieves a good standard of design and Policy PP30 states that the Council will expect development to preserve or enhance the historic, architectural and archaeological significance of heritage assets and their settings. Developments within conservation areas and affecting listed/locally listed buildings should enhance or better reveal the significance and value of the site within the street scene and wider setting and should seek to retain buildings that make a positive contribution to the conservation area.
25. The Town Centre Heritage SPD identifies weaknesses and threats to the character of the High Street, including poor quality maintenance of building fabric and the increase in vacant commercial units.
26. No 134 High Street is an attractive Victorian building and is a prominent feature of the Town Centre Heritage Conservation Area, due to its siting, scale and design. The Town Centre Heritage SPD states that it is a notable Italianate corner building in a key focal point; one of a number of 'good buildings from the Victorian period'; and a historic building of significance which contributes to the strong character of this part of the High Street. It is locally listed, due to its architectural interest.
27. The addition of 4th. floor dormer windows to the roof and the other proposed alterations to windows have all previously been approved under planning permission APP/17/01116/F. These alterations and additions were considered to respect and preserve the appearance of the locally listed building and would provide additional windows to habitable rooms within the proposed flats. The proposals also include lowered cill heights of three windows in the first-floor side (south-west) elevation and replacement windows, designed to replicate the appearance of the existing windows, to improve the energy efficiency of the flats. All of these proposed works would preserve and enhance the appearance of the locally listed building and its contribution to the character and appearance of the Conservation Area.
28. The proposed internal bin store and cycle store would be located to the rear of the building and would have no impact on the appearance of the building.
29. The proposals would enhance the setting of the Poole Town Centre Heritage Conservation Area and respect the appearance of the locally listed building, in accordance with Policies PP27 and PP30 of Poole Local Plan, the policies of the Poole Quays Forum Neighbourhood Plan and the Town Centre Heritage Conservation Area Appraisal and Management Plan SPD.

Impact on the neighbouring amenity:

30. There are residential properties at the opposite side of North Street but the proposed scheme would not result in material harm to the amenities of any neighbouring properties by reason of loss of light or outlook.
31. The proposed new windows to the side and front elevations would contribute to the mutual level of overlooking which is already established between the buildings but not such as would be materially harmful to the occupants of any other nearby shops, offices or homes.
32. The proposed flats would have no material impact on the church premises occupying the ground and first floors. Similarly, the activities associated with the established A3 and D1 uses on site would not materially harm the amenities of the prospective occupiers of the proposed flats. The relationship between the commercial and other uses and the proposed residential development would not be unusual in the Town Centre.
33. Given the town centre location and ready access to many services, facilities and leisure and recreation opportunities, the proposed scheme would provide an acceptable standard of amenity for the future occupants of the proposed flats.

Impact on highway safety and parking provision:

34. The proposed cycle parking to serve the six flats would accommodate six bikes. This is acceptable and can be secured by condition (#3). No car parking provision is proposed to serve the flats since the building occupies the whole of the application site.
35. The BCP Highway Authority had advised that five parking spaces would be required to meet the Council's adopted standards for the proposed flats and that the proposals would therefore result in a parking shortfall.
36. The application site is in the town centre within walking distance of the mainline railway and bus stations; shops; and other leisure and recreation facilities and close to cycle routes. Notwithstanding its highly sustainable location, the Highway Authority had advised that Poole has one of the highest household car ownership levels in the UK and that there is relatively high existing demand for parking in the area. The Council's Parking SPD car parking requirements are in any case zonal and are therefore already adjusted for the sustainability of the location. The lack of on site parking provision on site could therefore result in drivers taking short term parking risks; parking in inconsiderate locations; drivers trawling the streets looking for parking spaces; and increased vehicle manoeuvring onto the highways adjacent to the site. The proposals could therefore reduce highway safety in the area. The Highway Authority therefore recommended refusal of the proposed scheme as contrary to Policies PP27 and PP35 of the Poole Local Plan.
37. The NPPF seeks to promote sustainable development and includes the environmental objective of seeking to "contribute to protecting and enhancing the

natural, built and historic environment; including making effective use of the land, helping to improve biodiversity, using natural resources prudently, minimising waste and pollution, and mitigating and adapting to climate change, including to a low carbon economy” (paragraph 8c). The proposals would potentially deliver development which could help reduce pollution and vehicular movements in Town Centre and promote more sustainable modes of transport as a response to climate change.

38. Whilst the prospective occupiers of the flats could not be prevented from owning a car, the proposals would make this less convenient and would encourage alternative modes of transport. In this respect the proposals are similar to other homes on High Street, where limited or zero on-site parking provision is typical. There are nevertheless public car parking opportunities close to the application site. Moreover, both the potential for trip generation and car parking requirements associated with six flats are likely to be less than those associated with the converted floor space were it to instead be occupied by Class D1 users, which might include medical services; day nurseries; and for the provision of education.
39. The environmental, social and economic benefits of the proposed scheme would therefore, on balance, outweigh the lack of on-site parking in this particular case.

Implications of the proposal within the future flood zone:

40. The application site is not currently at risk of flooding, but is within an area at risk of future flooding and could be subject to flooding over the life of the development. The town centre has however been identified as appropriate for increased development due to its sustainability and the expectation of flood defences. The building has a ground floor entrance at 2.4m AOD with all of the proposed habitable accommodation being at or above the first floor level of 6.5m AOD. Flood resiliency measures would be required up to 3.6m AOD and these can be secured by condition (#4) to ensure that the proposals would comply with the provision of Policy PP38 of the Poole Local Plan.

Sustainability Issues:

41. It will be possible for the proposed development to incorporate renewable energy equipment to provide 10% of estimated energy use in accordance with PP37. This can be secured by condition (#5).

SAMM and CIL Compliance:

Contributions Required			Dorset Heathland SAMM	Poole Harbour Recreation SAMM
Flats	Existing	0	@ £269	@ £96
	Proposed	6		
	Net increase	6	£1614	£576
Total Contributions			£1614 (plus admin fee)	£576 (plus admin fee)
CIL	Zone A		@ £230sq m	

42. Mitigation of the impact of the proposed development on recreational facilities; Dorset Heathlands and Poole Harbour Special Protection Areas; and strategic transport infrastructure is provided for by the Community Infrastructure Levy (CIL) Charging Schedule adopted by the Council in February 2019. In accordance with CIL Regulation 28 (1) this confirms that dwellings are CIL liable development and are required to pay CIL in accordance with the rates set out in the Council's Charging Schedule.

43. The site is within 5km (but not within 400m) of Heathland SSSI and the proposed net increase in dwellings would not be acceptable without appropriate mitigation of their impact upon the Heathland. As part of the Dorset Heathland Planning Framework a contribution is required from all qualifying residential development to fund Strategic Access Management and Monitoring (SAMM) in respect of the internationally important Dorset Heathlands. This proposal requires such a contribution, without which it would not satisfy the appropriate assessment required by the Habitat Regulations

44. In addition, the proposed net increase in dwellings would not be acceptable without appropriate mitigation of their recreational impact upon the Poole Harbour SPA and Ramsar site. A contribution is required from all qualifying residential development in Poole to fund Strategic Access Management and Monitoring (SAMM) in respect of the internationally important Poole Harbour. This proposal requires such a contribution, without which it would not satisfy the appropriate assessment required by the Habitat Regulations.



45. The applicant has submitted a Section 111 form and has paid the relevant contributions towards Dorset Heathlands and Poole Harbour Recreation SAMM.

### **Summary and Planning Balance**

46. The proposals would provide six flats in a highly sustainable Town Centre location in a manner which would respect the appearance of the locally listed building and the streetscene, preserving and enhancing the character of the Town Centre Heritage Conservation Area. Vacant floor space within the town centre would be brought back into active use, contributing to the vitality of High Street and the Town Centre whilst also retaining the church and cafe uses already in the building and maintaining the amenities of the occupants of these and of other nearby residential; retail; and commercial units. The proposal could also contribute to tackling climate change by reducing the carbon footprint and pollution from vehicular movements in the town centre where access to sustainable modes of transport is both convenient and encouraged.
47. The proposals would nevertheless also result in a shortfall of car parking on site, which could give rise to increased highway safety dangers; impact on the flow of traffic; and increased congestion in the vicinity of the site
48. Given the heritage and environmental merits of the proposals, as set out above, and that they could help deliver the economic; social; and environmental objectives of sustainable development, in line with the adopted Local Plan policies and the provisions of the NPPF then, notwithstanding the identified shortfall in car parking provision and the identified consequences of that, the proposals are recommended for approval.

### **RECOMMENDATION**

**GRANT permission with the following conditions, which are subject to alteration/addition by the Head of Planning and Building Control provided any alteration/addition does not go to the core of the decision and the completed S111 agreement:**

1. GN150 (Time Expiry 3 Years (Standard))
2. GN040 (Match Materials to the Existing Building)
3. HW110 (Cycle Provision)
4. AA01 (Non standard Condition)  
Construction works shall be undertaken in accordance with the approved plans and details of preventive flood resilience measures contained in the section 9.05 of the Design and Access Statement (Issue 5, received 14/02/20).

Reason -

In order that the Council is satisfied with the details of the proposal having regard to the existing and adjacent site levels and in accordance with Policy PP38 of the

Poole Local Plan (November 2018).

5. GN162 (Renewable Energy - Residential) (10%)

6. PL01 (Plans Listing)

7. GN120 (Storage of Refuse)

### **Informative Notes**

1. IN72 (Working with applicants: Approval)

3. IN74 (Community Infrastructure Levy - Approval)

4. IN81 (SAMM Approval)

5. IN84 (AA passed)

6. IN00 (Non Standard Informative)

Safeguards should be implemented during the construction phase to minimise the risks of pollution and detrimental effects to the water interests in and around the site. Such safeguards should cover the use of plant and machinery, oils/chemicals and materials; the use and routing of heavy plant and vehicles; the location and form of work and storage areas and compounds and the control and removal of spoil and wastes. The applicant is referred to the Environment Agency's Pollution Prevention Guidelines, which can be found at:  
<https://www.gov.uk/guidance/pollution-prevention-for-businesses>

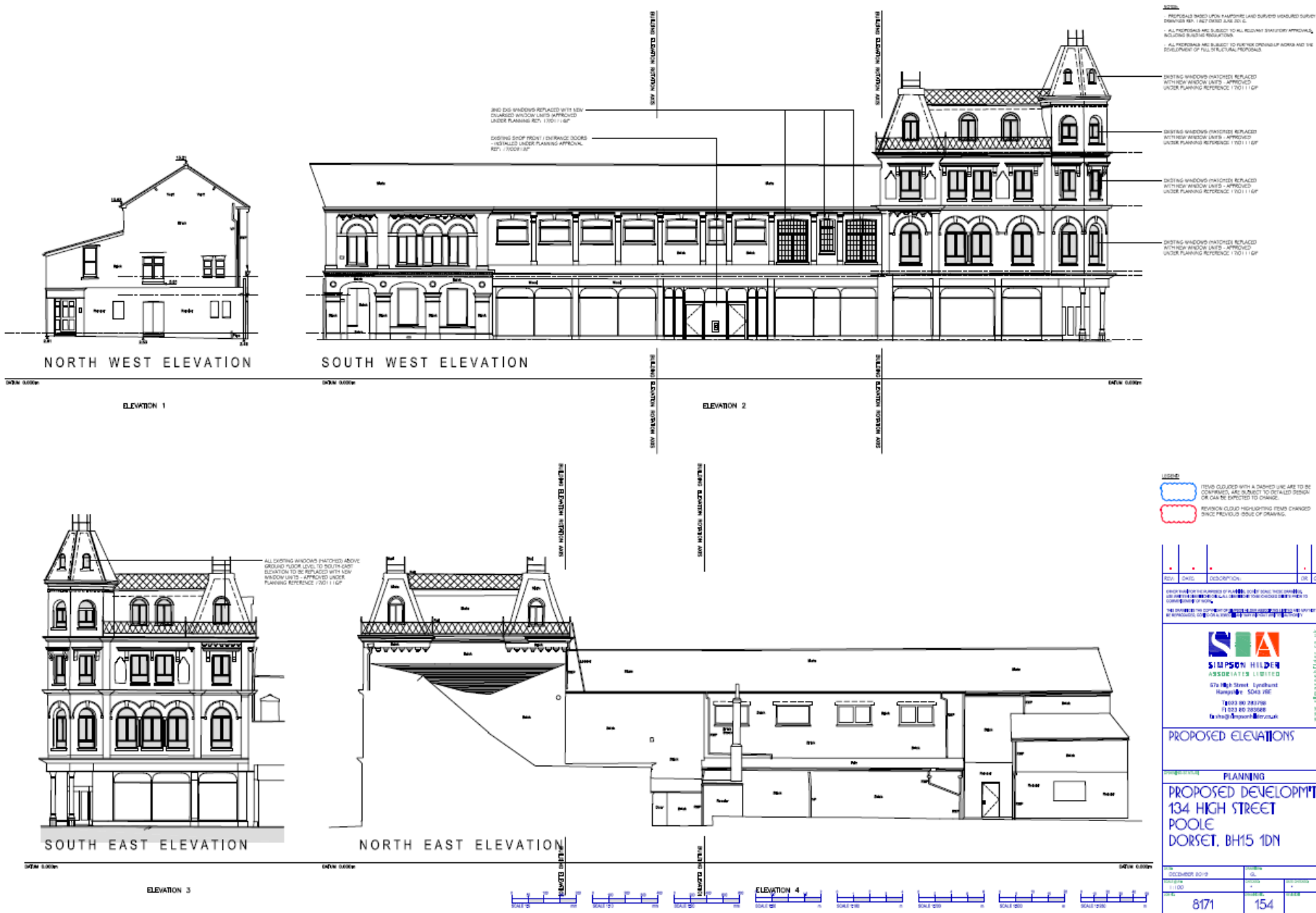
7. IN00 (Non Standard Informative)

If any controlled waste is to be removed off site, then site operator must ensure a registered waste carrier is used to convey the waste material off site to a suitably authorised facility. If the applicant require more specific guidance it is available on the environment Agency's website  
<https://www.gov.uk/how-to-classify-different-types-of-waste>

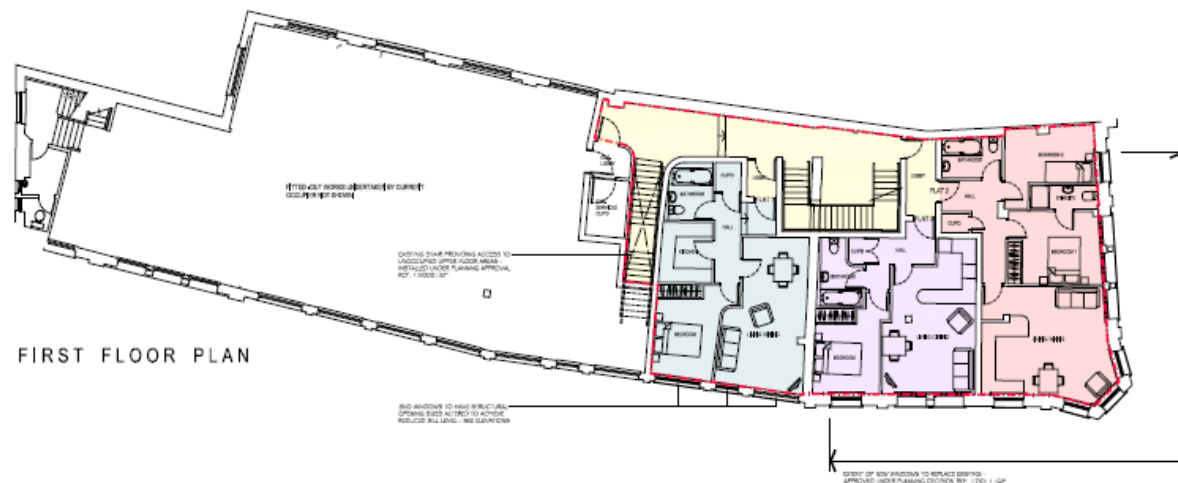
BLOCK PLAN

ITEMS CLOUDED WITH A DASHED LINE ARE TO BE CONFIRMED, ARE SUBJECT TO DETAILED DESIGN OR CAN BE EXPECTED TO CHANGE.

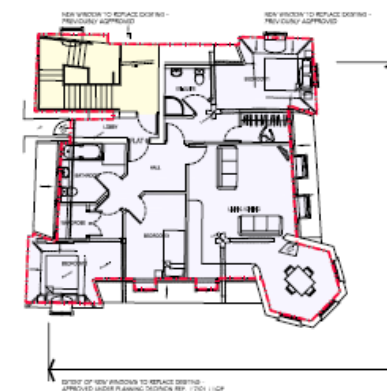
REVISION CLOUD HIGHLIGHTING ITEMS CHANGED SINCE PREVIOUS ISSUE OF DRAWING.



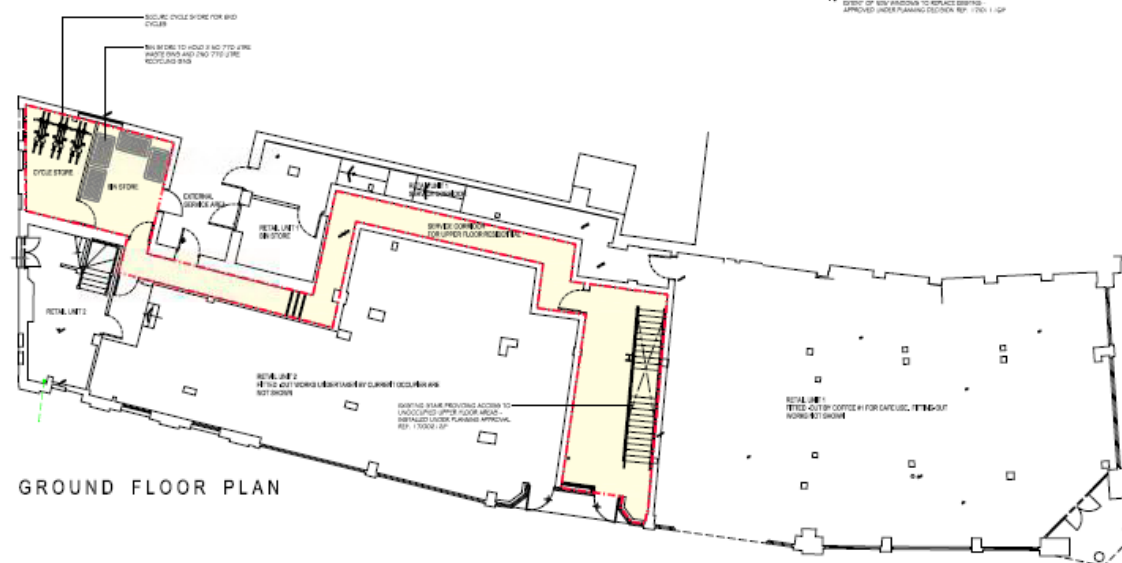
7



FIRST FLOOR PLAN



THIRD FLOOR PLAN



GROUND FLOOR PLAN



SECOND FLOOR PLAN



REF.	DATE	DESCRIPTION	BY
<p>DESIGNED BY THE ARCHITECTS OF 100-102, 104-106, 108-110, 112-114, 116-118, 120-122, 124-126, 128-130, 132-134, 136-138, 140-142, 144-146, 148-150, 152-154, 156-158, 160-162, 164-166, 168-170, 172-174, 176-178, 180-182, 184-186, 188-190, 192-194, 196-198, 200-202, 204-206, 208-210, 212-214, 216-218, 220-222, 224-226, 228-230, 232-234, 236-238, 240-242, 244-246, 248-250, 252-254, 256-258, 260-262, 264-266, 268-270, 272-274, 276-278, 280-282, 284-286, 288-290, 292-294, 296-298, 300-302, 304-306, 308-310, 312-314, 316-318, 320-322, 324-326, 328-330, 332-334, 336-338, 340-342, 344-346, 348-350, 352-354, 356-358, 360-362, 364-366, 368-370, 372-374, 376-378, 380-382, 384-386, 388-390, 392-394, 396-398, 400-402, 404-406, 408-410, 412-414, 416-418, 420-422, 424-426, 428-430, 432-434, 436-438, 440-442, 444-446, 448-450, 452-454, 456-458, 460-462, 464-466, 468-470, 472-474, 476-478, 480-482, 484-486, 488-490, 492-494, 496-498, 500-502, 504-506, 508-510, 512-514, 516-518, 520-522, 524-526, 528-530, 532-534, 536-538, 540-542, 544-546, 548-550, 552-554, 556-558, 560-562, 564-566, 568-570, 572-574, 576-578, 580-582, 584-586, 588-590, 592-594, 596-598, 600-602, 604-606, 608-610, 612-614, 616-618, 620-622, 624-626, 628-630, 632-634, 636-638, 640-642, 644-646, 648-650, 652-654, 656-658, 660-662, 664-666, 668-670, 672-674, 676-678, 680-682, 684-686, 688-690, 692-694, 696-698, 700-702, 704-706, 708-710, 712-714, 716-718, 720-722, 724-726, 728-730, 732-734, 736-738, 740-742, 744-746, 748-750, 752-754, 756-758, 760-762, 764-766, 768-770, 772-774, 776-778, 780-782, 784-786, 788-790, 792-794, 796-798, 800-802, 804-806, 808-810, 812-814, 816-818, 820-822, 824-826, 828-830, 832-834, 836-838, 840-842, 844-846, 848-850, 852-854, 856-858, 860-862, 864-866, 868-870, 872-874, 876-878, 880-882, 884-886, 888-890, 892-894, 896-898, 900-902, 904-906, 908-910, 912-914, 916-918, 920-922, 924-926, 928-930, 932-934, 936-938, 940-942, 944-946, 948-950, 952-954, 956-958, 960-962, 964-966, 968-970, 972-974, 976-978, 980-982, 984-986, 988-990, 992-994, 996-998, 1000-1002, 1004-1006, 1008-1010, 1012-1014, 1016-1018, 1020-1022, 1024-1026, 1028-1030, 1032-1034, 1036-1038, 1040-1042, 1044-1046, 1048-1050, 1052-1054, 1056-1058, 1060-1062, 1064-1066, 1068-1070, 1072-1074, 1076-1078, 1080-1082, 1084-1086, 1088-1090, 1092-1094, 1096-1098, 1100-1102, 1104-1106, 1108-1110, 1112-1114, 1116-1118, 1120-1122, 1124-1126, 1128-1130, 1132-1134, 1136-1138, 1140-1142, 1144-1146, 1148-1150, 1152-1154, 1156-1158, 1160-1162, 1164-1166, 1168-1170, 1172-1174, 1176-1178, 1180-1182, 1184-1186, 1188-1190, 1192-1194, 1196-1198, 1200-1202, 1204-1206, 1208-1210, 1212-1214, 1216-1218, 1220-1222, 1224-1226, 1228-1230, 1232-1234, 1236-1238, 1240-1242, 1244-1246, 1248-1250, 1252-1254, 1256-1258, 1260-1262, 1264-1266, 1268-1270, 1272-1274, 1276-1278, 1280-1282, 1284-1286, 1288-1290, 1292-1294, 1296-1298, 1300-1302, 1304-1306, 1308-1310, 1312-1314, 1316-1318, 1320-1322, 1324-1326, 1328-1330, 1332-1334, 1336-1338, 1340-1342, 1344-1346, 1348-1350, 1352-1354, 1356-1358, 1360-1362, 1364-1366, 1368-1370, 1372-1374, 1376-1378, 1380-1382, 1384-1386, 1388-1390, 1392-1394, 1396-1398, 1400-1402, 1404-1406, 1408-1410, 1412-1414, 1416-1418, 1420-1422, 1424-1426, 1428-1430, 1432-1434, 1436-1438, 1440-1442, 1444-1446, 1448-1450, 1452-1454, 1456-1458, 1460-1462, 1464-1466, 1468-1470, 1472-1474, 1476-1478, 1480-1482, 1484-1486, 1488-1490, 1492-1494, 1496-1498, 1500-1502, 1504-1506, 1508-1510, 1512-1514, 1516-1518, 1520-1522, 1524-1526, 1528-1530, 1532-1534, 1536-1538, 1540-1542, 1544-1546, 1548-1550, 1552-1554, 1556-1558, 1560-1562, 1564-1566, 1568-1570, 1572-1574, 1576-1578, 1580-1582, 1584-1586, 1588-1590, 1592-1594, 1596-1598, 1600-1602, 1604-1606, 1608-1610, 1612-1614, 1616-1618, 1620-1622, 1624-1626, 1628-1630, 1632-1634, 1636-1638, 1640-1642, 1644-1646, 1648-1650, 1652-1654, 1656-1658, 1660-1662, 1664-1666, 1668-1670, 1672-1674, 1676-1678, 1680-1682, 1684-1686, 1688-1690, 1692-1694, 1696-1698, 1700-1702, 1704-1706, 1708-1710, 1712-1714, 1716-1718, 1720-1722, 1724-1726, 1728-17</p>			

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## PLANNING COMMITTEE

Application Address	15 Creedy Drive Christchurch BH23 1NX
Proposal	Single storey rear extensions. New decking with balustrade
Application Number	8/20/0137/HOU
Applicant	Mr N Brown
Agent	Mr Matt Stevens
Date Application Valid	11 February 2020
Decision Due Date	7 April 2020
Extension of Time Date (if applicable)	
Ward	Christchurch Town
Report status	Public
Meeting date	21 May 2020
Recommendation	Grant, in accordance with the Recommendation within the report.
Reason for Referral to Planning Committee	Application called to Committee by Cllr P Hall as <ul style="list-style-type: none"> <li>it may form a cramped development and may be contrary to policy H12 1 &amp; 2.</li> </ul>
Case Officer	Charlotte Dayson-Smith

### Description of Development

1. Single storey rear extension. New decking with balustrade.

### Key Issues

2. The main considerations involved in this application are:
  - Impact on the character and appearance of the area
  - Impact on neighbours' living conditions

## Planning Policies

### 3. Development Plan:

KS1 Presumption in favour of sustainable development

HE2 Design of new development

H12 Residential Infill

ME6 Flood Management, Mitigation, and Defence

National Guidance

The guidance contained in the National Planning Policy Framework (NPPF) is a material consideration.

Section 7 – Requiring good design

Section 10 – Meeting the challenge of climate change, flooding and coastal change

Section 16 - Conserving and enhancing the historic environment

### 4. Relevant Planning Applications and Appeals

Application Number	Site Address	Proposal	Decision
8/20/0300/CLP	15 Creedy Drive	Single storey rear extension.	Lawful 28/05/2020
8/05/0298	9 Creedy Drive	Conservatory on rear elevation	Grant 23/06/2005
8/05/0296	7 Creedy Drive	Conservatory on rear elevation	Grant 23/06/2005
8/06/0443	25 Creedy Drive	Retrospective application for the erection of pitched roof conservatory to rear	Granted 29/09/2006
8/07/0450	11 Creedy Drive	Pitched roof conservatory to rear	Granted 22/08/2007
8/08/0207	32 Wick Point Mews	Erection of single storey extension and conservatory to rear of dwelling	Granted 09/05/2008
8/12/0271	24 Wick Point Mews	Erection of conservatory to rear	Granted 29/08/2012
8/17/3535/CLP	20 Wick Point Mews	Single storey rear extension and loft conversion	Lawful 10/01/2018
8/18/3390/HOU	26 Wick Point Mews	Rear extension and enlargement of roof to provide further accommodation	Granted 11/02/2019
8/19/1330/HOU	27 Creedy Drive	Single storey rear extension. Add windows to side elevation	Granted 10/01/2020



8/19/1455/CLP	15 Creedy Drive	Single storey rear extension and decking with balustrade	Not Lawful 08/01/2020
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## Representations

5. A site notice was posted near the site on 03/03/2020 with an expiry of 24/03/2020.
6. A total of 4 letters of representation were received raising the following issues;
  - Impact on residential amenities
  - Loss of light
  - Loss of privacy
  - Impact on complex drainage/sewerage on site
  - Noise and disturbance during construction and emanating from bi-fold doors and decking

## Consultations

7. Christchurch Town Council – no comments received.

## 8. Constraints

- Flood Zone 2
- Coastal Area (Policy)
- Wessex Water Sewer Flooding

## Planning Assessment

### Site and Surroundings

9. The application site is within a modern residential development within central Christchurch, a short distance from the Christchurch Central Conservation Area and The Quomps. The site is occupied by a 3 storey townhouse in red brick and slate and is the end property in a terrace of 3. The properties in the terrace are slightly staggered, with the end properties being 0.5m longer to the front and rear than the middle property. There have been no previous alterations to no.15 however no.19 has a 5m extension to the rear.

### Impact on character and appearance of the area

10. The proposal is a flat roof, single storey rear extension, 3.10m in height, which extends beyond the rear wall of the dwelling by 2m. There are no windows proposed in either side elevation and the rear is served by bifold doors out on to a proposed decking. The extension has a flat roof.

11. The proposed decking is 1.5m in depth with a glazed balustrade of approx. 1.3m in height, the decking is 0.3m high and runs the whole width of the extension, (approx. 6m).
12. The extension is to be constructed to the rear of the property and will not be visible from the streetscene therefore it is considered to have a minimal impact on the character of the area.
13. The proposed extension is to be constructed from materials which match those of the existing building and is, therefore, considered to be a subservient and well-designed minor addition to the dwelling.
14. The proposal requires planning permission only because the extension is 0.1m over the height limitation for a rear extension in Part 1, Class A of The Town and Country Planning (General Permitted Development) (England) Order 2015 (GPDO). A reduction in height of 0.1m of the extension would make this element permitted development.
15. Several properties in the immediate area have erected similar single storey extensions, both erected with and without planning permission; as referenced above.

#### Impact on neighbours' living conditions

16. The proposed extension is single storey and flat roofed, its scale and height and limited projection of 2m would minimise general disturbance to amenity for adjoining properties in accordance with Policy HE2. The scheme is considered to be compatible in its relationship to nearby properties. It would not lead to significant impact on neighbouring living conditions in respect of a loss of daylight or outlook, nor would it have an overbearing impact.
17. The extension is to be built adjoining the boundary with no.17 and there are no windows proposed to either side elevation. The property at 18 Wick Point Mews is some 10.4m from the extension and at this distance, bearing in mind the limited height and projection to the rear, any impacts to the outlook from this neighbour would be minor. It is noted this property has extended to the rear although there is no record on the Council's systems of planning permission having been granted or its lawfulness having been assessed.
18. As noted above, if the height of the extension is reduced by 10cm to 3m, it is permitted development. In addition, the extension could then project 3m from the rear of the property and still be permitted development. The applicant therefore has a fallback position to erect a rear extension with greater impacts on the neighbouring properties than the current proposal.
19. In addition, the applicant would be entitled to apply under the Prior Notification procedure under Class A, Part G of GPDO to erect a rear extension projecting up to 6m (the current proposal is 2m) from the rear. This would be subject to a notification process and in the event of any objections being received, an assessment of the impacts on neighbouring amenity by the LPA.

20. The applicant has confirmed that a 3m high extension, projecting 2m from the rear elevation would be a lawful development under 8/20/0300/CLP in the Table in paragraph 4 above.
21. The Council in determining this application must give due weight to the applicant's fallback position to be able to erect a largely identical (and larger) extension as permitted development.
22. The proposed decking cannot be seen from either adjacent property, it is 0.3m in height and therefore is permitted development, under Class E of the GPDO. The applicant therefore has a fallback position to carry out these works without planning permission from the Local Planning Authority.
23. There is sufficient garden space so that the extension and decking can be accommodated without harm to the amenity of future occupiers.

### **Flood Risk**

24. The site lies in a current area of flood risk (Zone 2) and within future areas at risk of flooding. The applicant has provided a householder flood risk assessment which confirms that floor levels have been set no lower than existing and flood resilience measures incorporated. The scheme thereby complies with Policy ME6 in minimising flood risk.

### **Summary**

25. Each application must be considered on its own merits. The proposal involves a minor alteration to an existing dwellinghouse in the urban area which is acceptable in principle. If the extension is reduced in height by 10cm it does not require planning permission. A larger extension than that proposed can be erected as permitted development.
26. The development is compatible with its surroundings in its relationship to nearby properties including minimising general disturbance to amenity and thereby complies with Policy HE2.
27. The impact on the character and appearance of the area is minimal. Flood risk issues have been addressed.

### **Planning Balance**

28. The proposals would preserve the character and appearance of the area. The proposals have an acceptable impact neighbouring living conditions. The scheme will bring economic benefits during the construction stage. There is no harm caused by the proposals which demonstrably outweigh the benefits of the scheme. The applicant has a fallback position to erect a larger extension as permitted development.
29. The scheme is acceptable.

## **RECOMMENDATION**

Grant, subject to the following conditions which are subject to alteration/addition by the Head of Planning providing any alteration/addition does not go the core of the decision:

### **Conditions:**

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of this permission.

Reason: This condition is required to be imposed by Section 91 of the Town and Country Planning Act 1990.

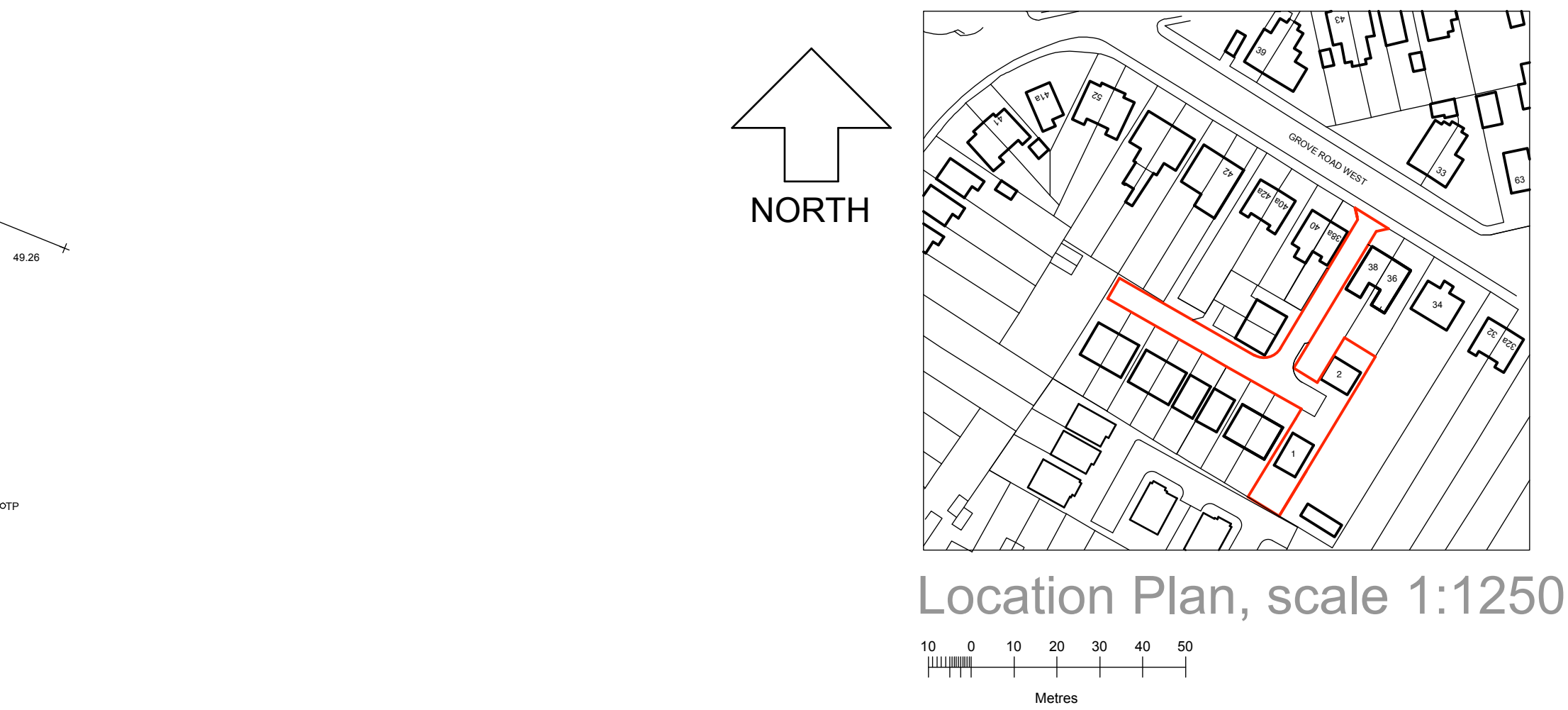
2. The development hereby permitted shall be carried out in accordance with the following approved plans:

ASP.19.090.002 A AMENDED Site Plan  
ASP.19.090.100 A AMENDED Proposed Plans  
ASP.19.090.200 A AMENDED Proposed Elevations  
ASP.19.090.201 A AMENDED Proposed Elevations

Reason: For the avoidance of doubt and in the interests of proper planning.

3. The materials and finishes to be employed on the external faces of the development, hereby permitted, shall match those of the existing building unless otherwise first agreed in writing by the Local Planning Authority.

Reason: To ensure satisfactory visual relationship of the new development to the existing.



Note:  
Do not scale from this drawing. The contractor is to check all dimensions on site.

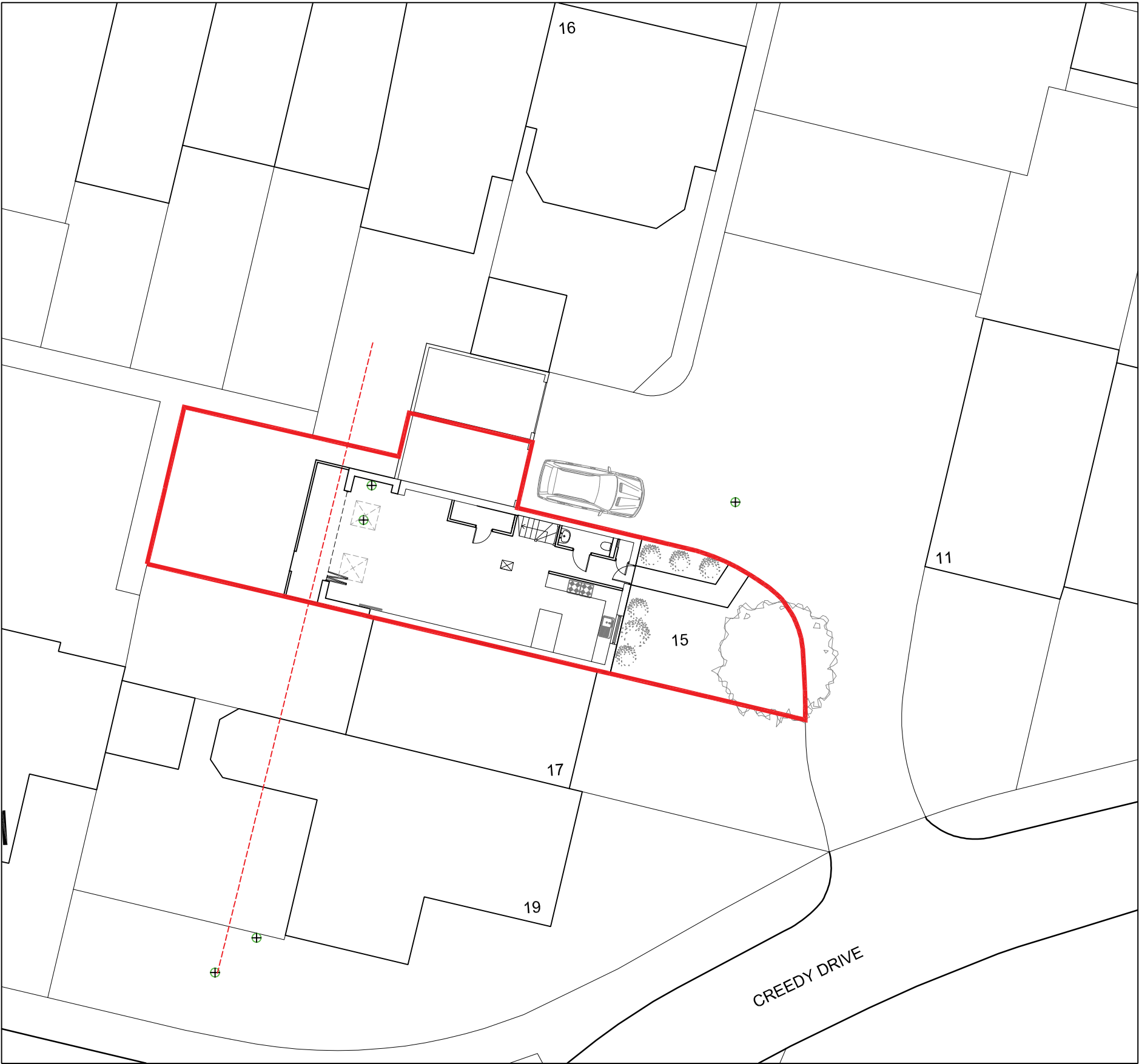
Residential Development  
r/o 36 Grove Road West  
Christchurch  
Dorset

2018-26-01  
Site/Block/Location Plans  
13 December 2018  
scale as shown

**Evans & Traves**  
Architecture and Town Planning

Evans & Traves LLP  
Architecture and Town Planning Consultancy  
8 Dewlands Road  
Venwood  
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e info@evansandtraves.com





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  3. Contractors must verify all dimensions and report any discrepancies before putting work in hand or making any shop drawings.
  4. Lower ground construction/ retaining structure to be structural engineers design.
  5. Electrical layouts to be agreed with client & added to drawing
  6. Drawings to be read in conjunction with specification.

----- Drainage path

A	08.04.20	Minor amendment made to proposed balustrade	CW
Rev	Date	Description	By
REVISIONS:			

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Proposed Site Plan

Scale @ A3: 1:200	Drawn by : SR	
Date : 11/09/19	Checked by : MS	
DRAWING No: ASP.19.090.002		REVISION: A

ARCHITECTURE - PLANNING - PROJECT MANAGEMENT - CDM

17a High Street, Christchurch,  
Dorset, BH23 1AB  
01202 473222  
[www.aspirearchitects.co.uk](http://www.aspirearchitects.co.uk)  
[info@aspirearchitects.co.uk](mailto:info@aspirearchitects.co.uk)  
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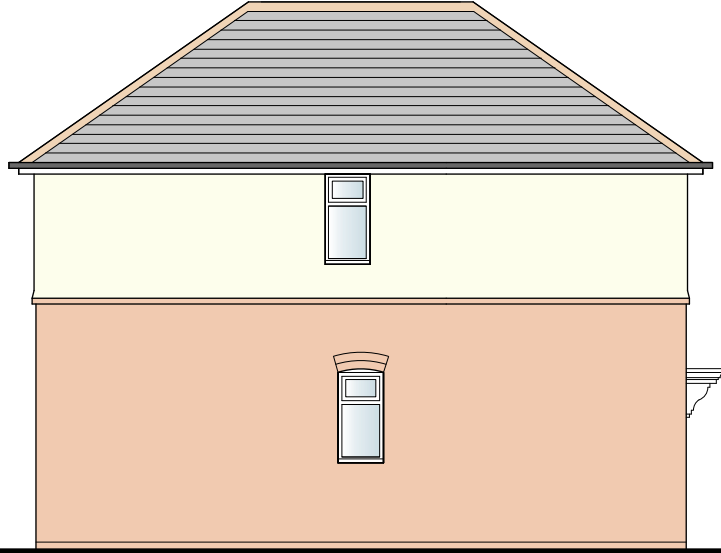


Aspire  
Architects

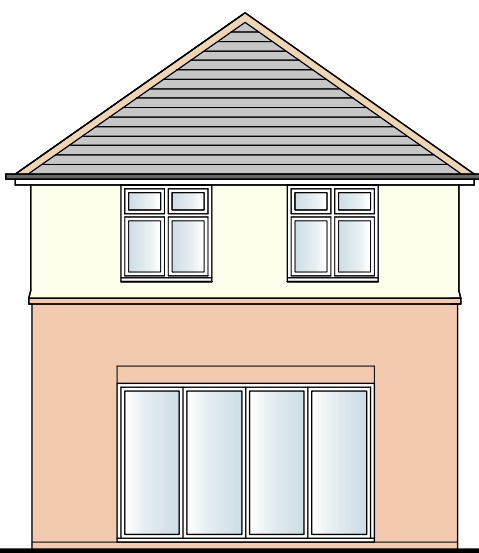
Proposed Site Plan 1:200  
0 20m



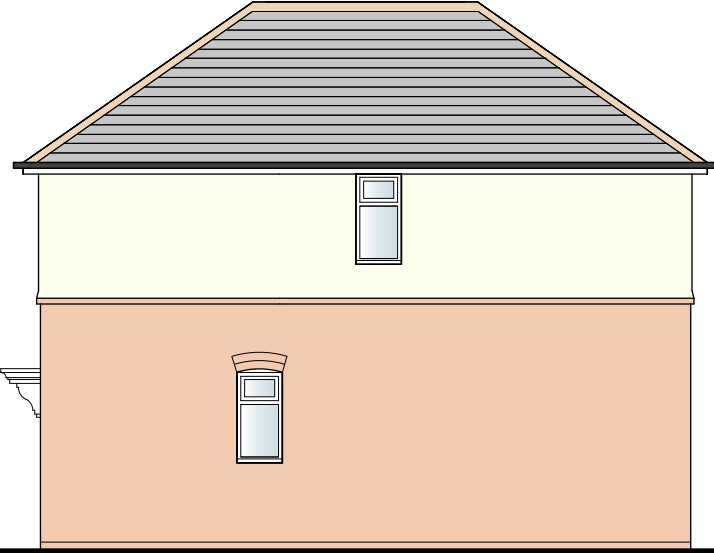
North Elevation



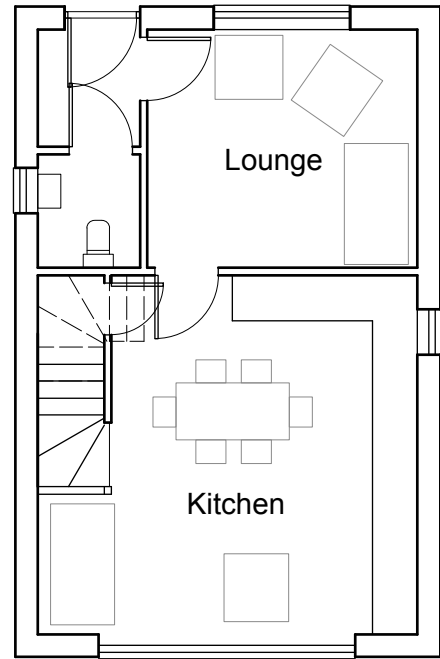
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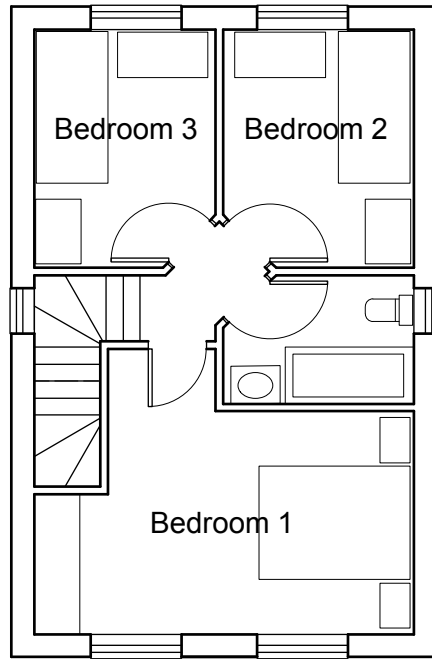
South Elevation



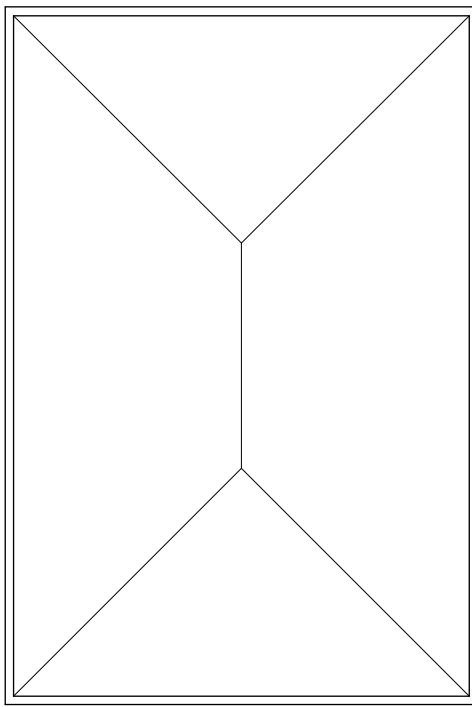
West Elevation



Ground Floor Plan



First Floor Plan



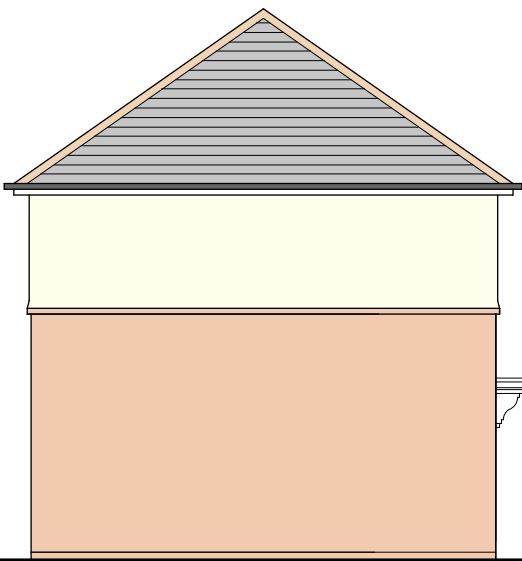
Roof Plan

# PLOT 1

Floor area = 80.36sq.m. (864sq.ft.)



South Elevation

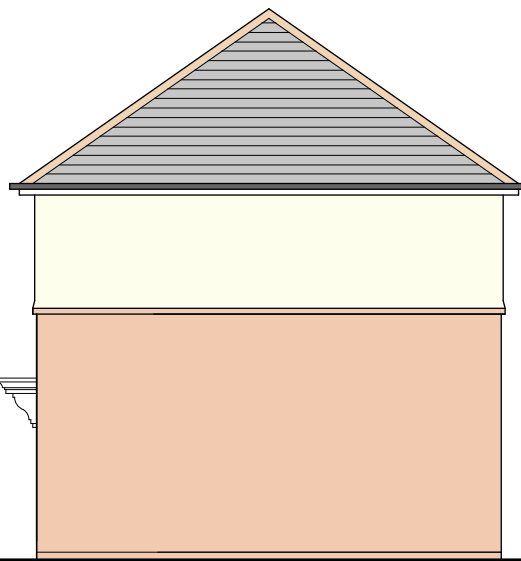


West Elevation

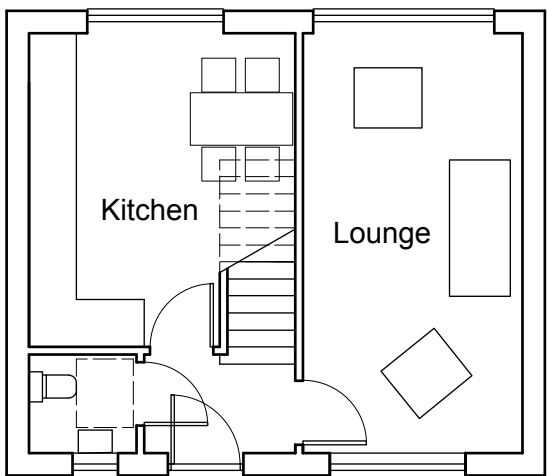


North Elevation

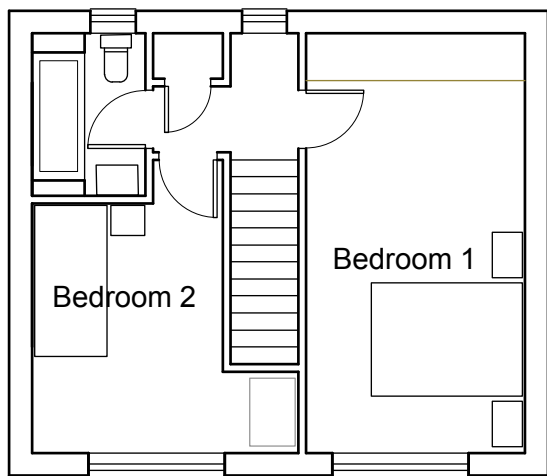
Obscured glazed bathroom & landing windows. Bottom light fixed shut, top light min. 1.7m above floor level.



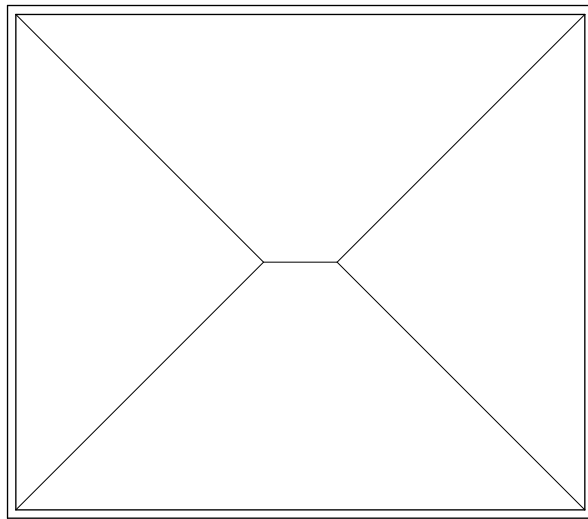
East Elevation



Ground Floor Plan



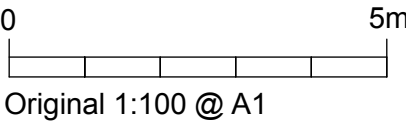
First Floor Plan



Roof Plan

# PLOT 2

Floor area = 72.42sq.m. (779sq.ft.)



Note:  
Do not scale from this drawing. The contractor is to check all dimensions on site.

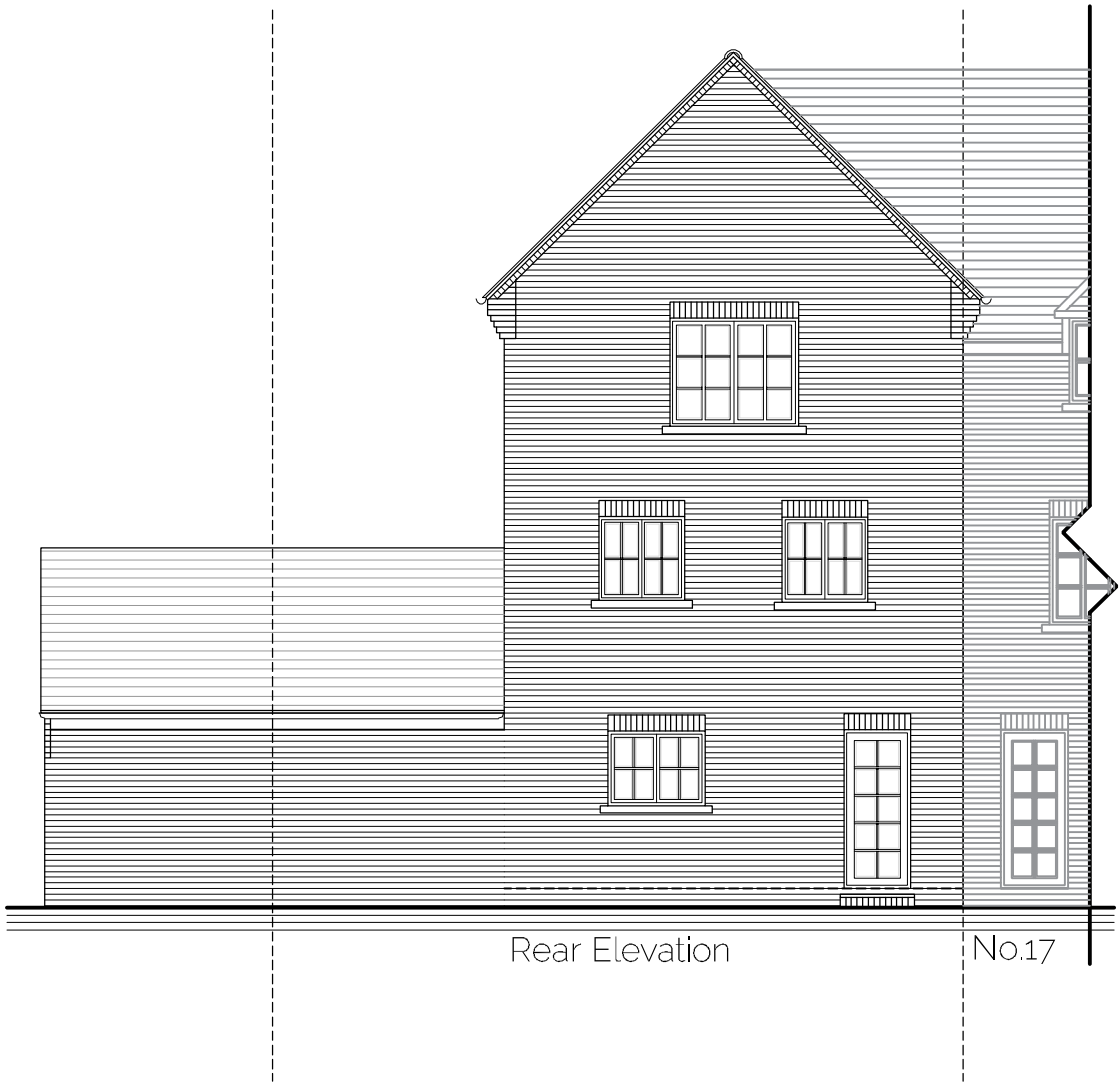
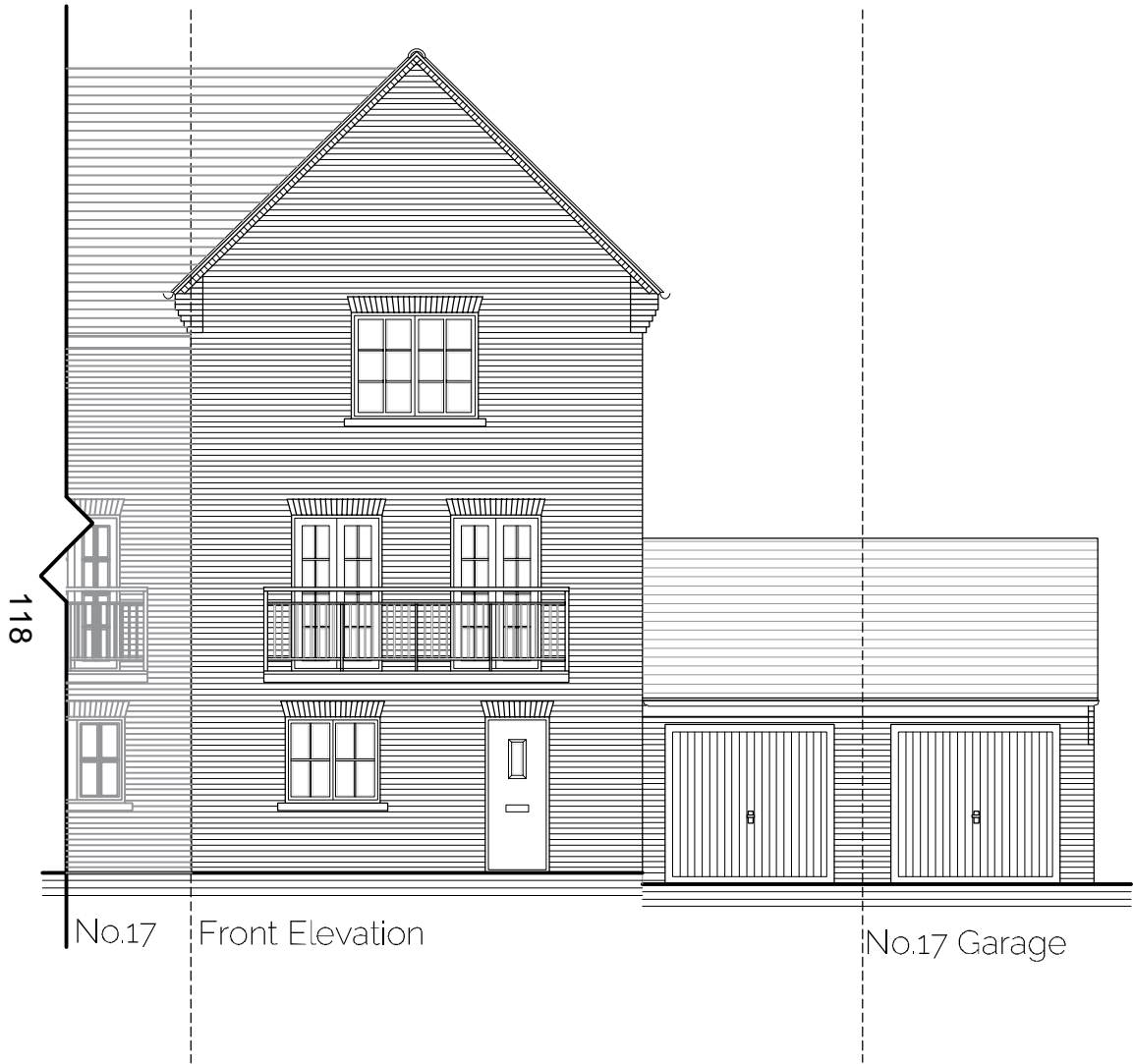
Residential Development  
r/o 36 Grove Road West  
Christchurch  
Dorset

2018-26-02	Rev.
Floor Plans & Elevations	
13 December 2018	Scale 1:100

**Evans & Traves**  
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  5. Electrical layouts to be agreed with client & added to drawing
  6. Drawings to be read in conjunction with specification.



A	05.04.20	Amendments made to No.17	Cw/
Rev	Date	Description	By PM

**REVISIONS:**

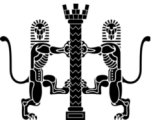
**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

<b>PROJECT &amp; DRAWING TITLE:</b>		
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Scale @ A3:1:100 Date: 20.05.19	Drawn by: SH Checked by: MS	
DRAWING No: ASP.19.090.004	REVISION: A	

ARCHITECTURE - PLANNING - PROJECT MANAGEMENT - CDM

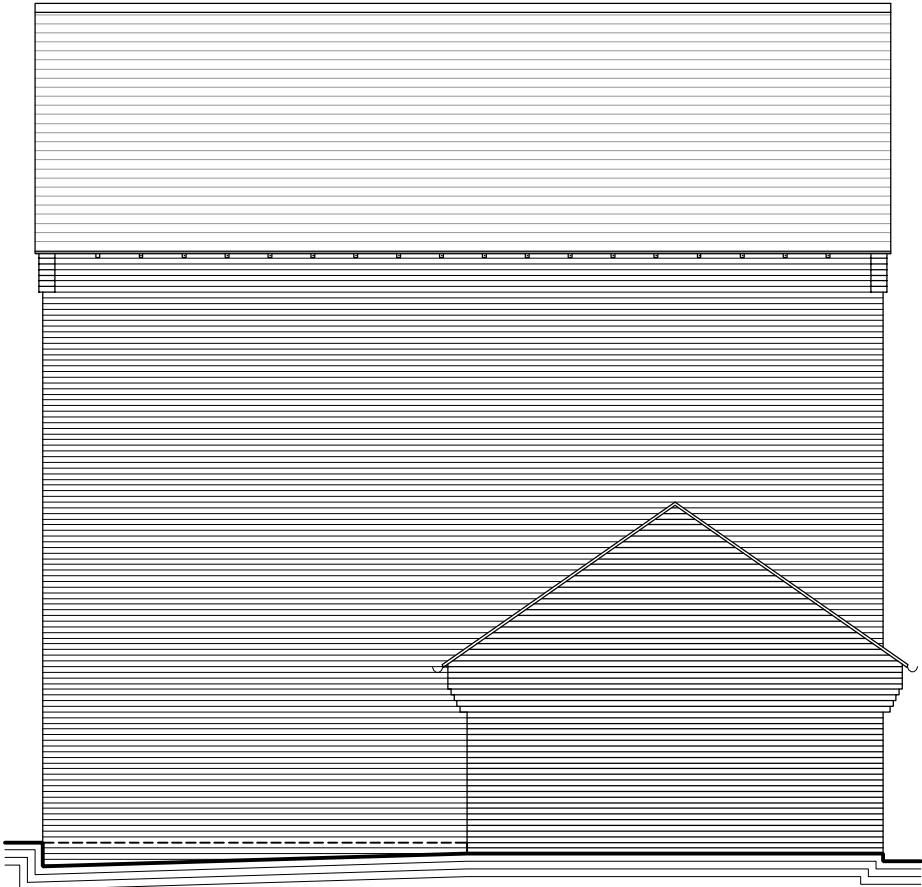
17a High Street, Christchurch  
Dorset, BH23 1AB  
01202 477222  
www.aspirearchitects.co.uk  
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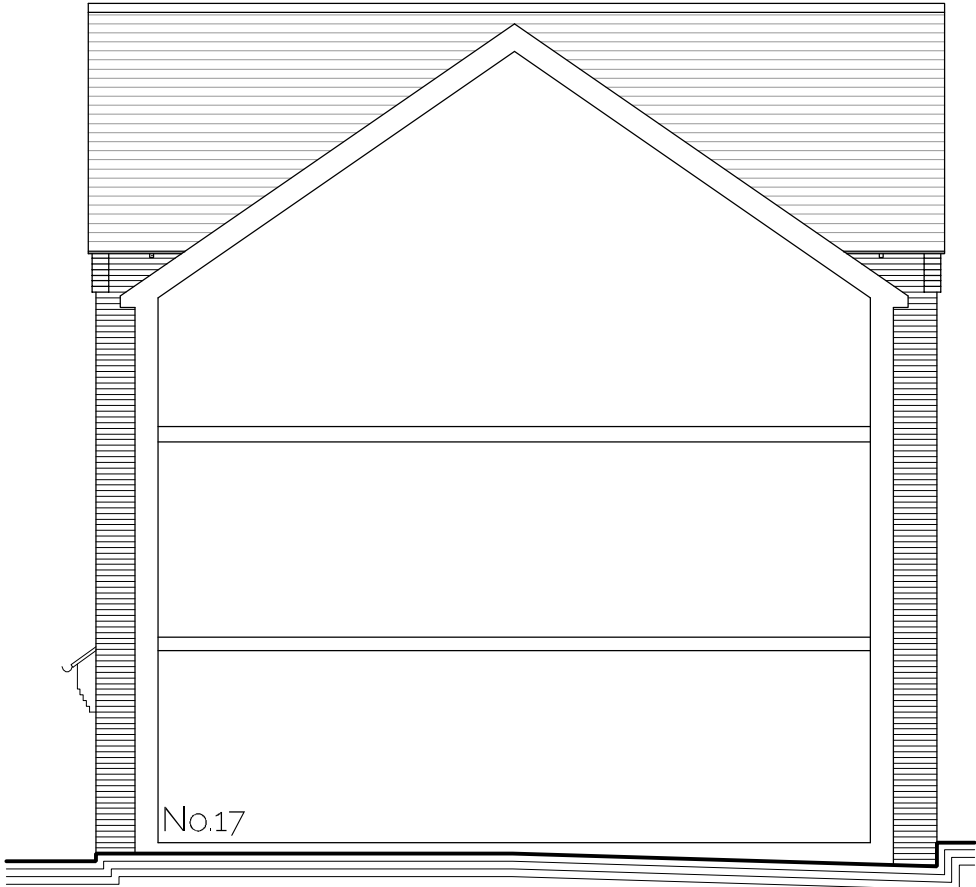
Existing Elevations 1:100  
0 10m



119



Side Elevation



Side Elevation

- NOTES:**
1. The contents of this drawing are copyright.
  2. Scaled drawing for Planning purposes only.
  3. Contractors must verify all dimensions and report any discrepancies before putting work in hand or making any shop drawings.
  4. Lower ground construction/ retaining structure to be structural engineers design.
  5. Electrical layouts to be agreed with client & accord to drawing
  6. Drawings to be read in conjunction with specification.

A	05.04.20	Additional elevation added for clarity	CW
Rev	Date	Description	By PM

REVISIONS:

PROJECT STAGE: PLANNING

CLIENT:  
Mr Brown

PROJECT & DRAWING TITLE:

15 Creedy Drive, Christchurch  
Existing Elevations

Scale @ A3:1:100	Drawn by : SH	
Date : 05.05.19	Checked by : MS	
DRAWING No:	ASP.19.090.005	REVISION:
		A

ARCHITECTURE - PLANNING - PROJECT MANAGEMENT - CDM

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Existing Elevations 1:100  
0 10m

120



Front Elevation



Rear Elevation

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  4. Lower ground construction/ retaining structure to be structural engineers design.
  5. Electrical layouts to be agreed with client & added to drawing
  6. Drawings to be read in conjunction with specification.

A	05.04.20	Minor Amendments made to No.17	CW
Rev	Date	Description	By PM

**REVISIONS:**

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Proposed Elevation

Scale @ A3:1:100	Drawn by : SR	REVISION: A
Date : 10.05/19	Checked by : MS	
DRAWING No: ASP.19.090.200		

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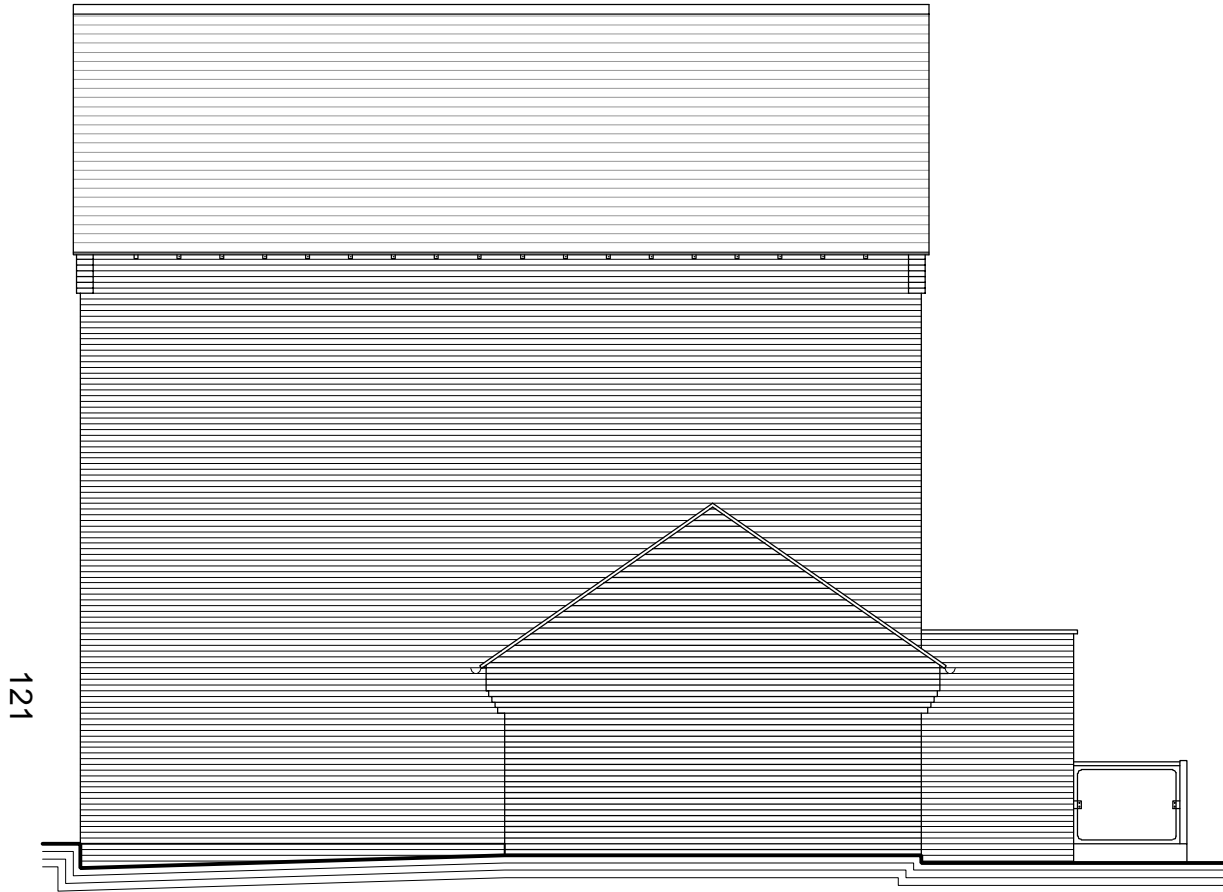


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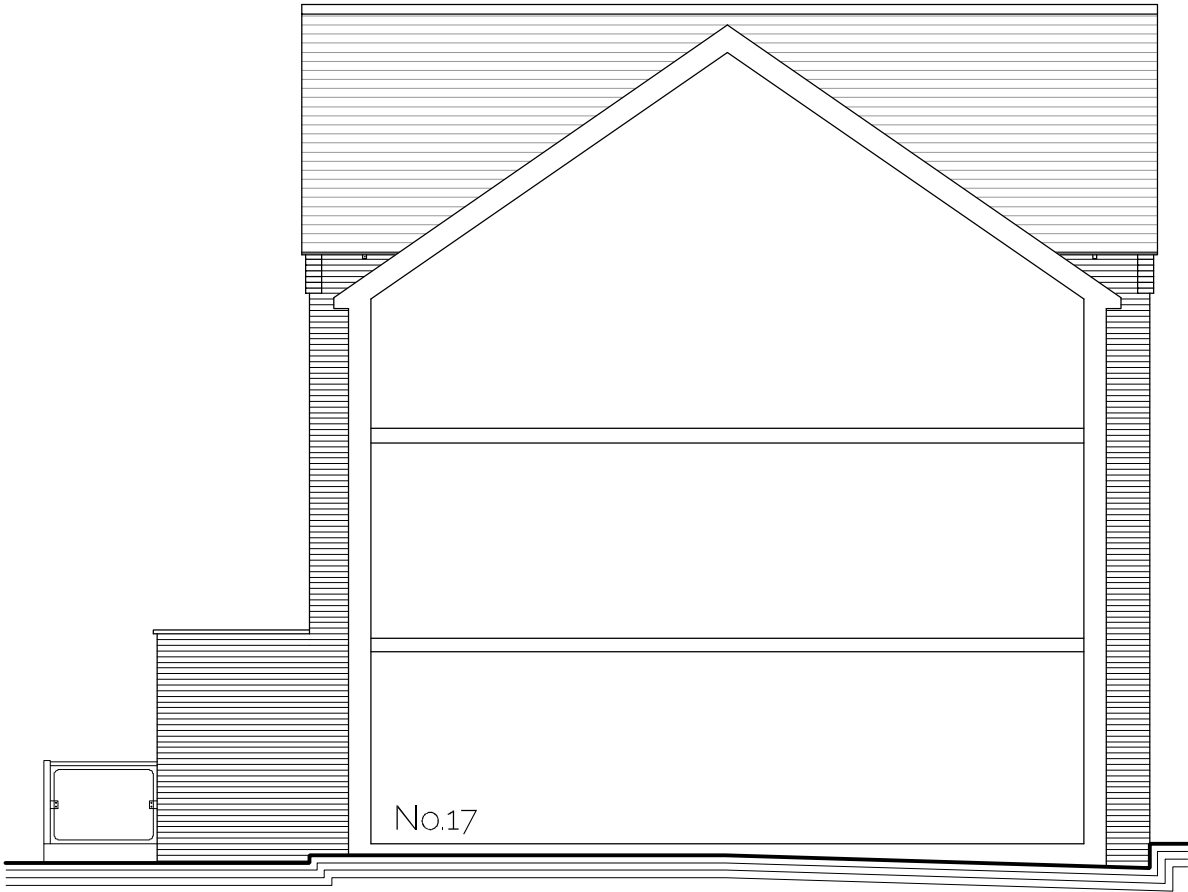
Proposed Elevations 1:100  
0

10m

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  2. Scaled drawing for Planning purposes only.
  3. Contractors must verify all dimensions and report any discrepancies before putting work in hand or making any shop drawings.
  4. Lower ground construction/ retaining structure to be structural engineers design.
  5. Electrical layouts to be agreed with client & added to drawing
  6. Drawings to be read in conjunction with specification.



Side Elevation



Side Elevation

A	08.04.20	Additional elevation added for clarity	CW	
Rev	Date	Description	By	PM

**REVISIONS:**

**PROJECT STAGE:** PLANNING

**CLIENT:**  
Mr Brown

**PROJECT & DRAWING TITLE:**

15 Creedy Drive, Christchurch  
Proposed Elevation

Scale @ A3:1:100	Drawn by: SR	
Date: 10.05/19	Checked by: MS	
DRAWING No:	ASP.19.090.201	REVISION:
		A

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Proposed Elevations 1:100



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